

**ADA Inventory Summary Report  
City of Fruitland**

**Prepared by: HDR**

**December 9, 2010**

# CONTENTS

|   |    |
|---|----|
| Introduction.....   | 3  |
| Purpose of Study.....   | 3  |
| Methodology.....  | 3  |
| Analysis of Pedestrian Ramps.....                                 | 4  |
| Analysis of Sidewalks and Driveway Approaches.....                | 5  |
| Cost Estimate.....  | 6  |
| Americans with Disabilities Act.....                              | 6  |
| Existing Conditions.....  | 8  |
| Pedestrian Ramps.....   | 8  |
| Sidewalks.....  | 12 |
| Driveway Approaches.....  | 13 |
| Recommendations.....  | 15 |
| General.....  | 15 |
| Provide Pedestrian Connectivity.....                              | 15 |
| Eliminate Obstructions.....                                       | 17 |
| Pedestrian Ramps.....   | 18 |
| Sidewalks.....  | 18 |
| Driveway Approaches.....  | 19 |
| Cost of Improvements.....   | 20 |
| Pedestrian Ramp Improvements.....                                 | 21 |
| Sidewalk Improvements.....  | 21 |
| Driveway Approach Improvements.....                               | 22 |
| Phasing of Improvements.....                                      | 22 |
| Summary.....  | 23 |
| Appendix A –Section Maps (1-5), Observed Intersections & Roadway  |    |
| Appendix B –Existing Conditions – Pedestrian Ramps Table          |    |
| Appendix C –Existing Conditions – Sidewalks Table                 |    |
| Appendix D –Section Maps (1-5), Non-Compliant Driveway Approaches |    |
| Appendix E –GPS Data Maps (1-2)                                   |    |
| Appendix F –Cost Estimates  |    |

## **Introduction**

It is known that some of the pedestrian facilities within the City of Fruitland are not in compliance with current ADA standards. At the request of the City of Fruitland, HDR conducted an inventory and evaluation of the existing pedestrian facilities to determine which facilities are non-compliant and the amount of work that must be completed to bring the City into compliance.

This report summarizes work conducted for the 2010 Fruitland ADA Inventory. Information on the condition of existing pedestrian facilities located throughout the City of Fruitland, Idaho, is provided. Additional information is provided on the data that was collected in the field, how the data was organized and evaluated, and the conclusions that were made. Photographs, GPS data, location maps and data tables are supplied in the appendices.

## **Purpose of Study**

The purpose of the Fruitland ADA Inventory is to identify and analyze the existing pedestrian facility conditions throughout the City of Fruitland with respect to ADA Compliance. The investigation and analysis is also intended to provide the City of Fruitland with an estimate of the cost to update existing pedestrian facilities to meet current ADA guidelines and to construct new ADA-compliant facilities where there currently are none.

## **Methodology**

Field work for the Fruitland ADA Inventory was collected on local roads within the City of Fruitland during the months of May and June, 2010, and evaluated in the HDR Boise office. Field work consisted of collecting data at 165 intersections, including 248 pedestrian ramps and along approximately 21.3 miles of roadway. The location of each intersection and pedestrian ramp and the roads that were observed can be found on the five Section Maps located in Appendix A.

The primary focus of the field work was to collect data on pedestrian ramps, sidewalks and driveway approaches. Various measurements and data collected at each pedestrian ramp include ramp running slope, ramp cross slope, landing slope, ramp dimensions, landing dimensions, width of the traversable path, the drop-off depth at the flow line and the presence of truncated domes. GPS data was recorded and compiled in GIS to show the locations where data was retrieved. Photos were taken and sorted by location and site sketches of the intersections and pedestrian ramps were produced. Data concerning the existing sidewalk conditions in Fruitland was also collected. This included measuring the sidewalk width, cross slope, running slope, and discontinuities or obstructions that may limit pedestrian mobility. Finally, driveway and commercial approaches were also surveyed. The presence of ramps and a traversable pathway around driveways and approaches was documented, as well as the approach slopes and widths.

To organize the collection of field data, the City of Fruitland has been divided into five sections (labeled Section 1 through Section 5 in Appendix A). Within each of these sections, the

observed intersections are alphabetically labeled. The corners of each intersection are numbered 1-4, as applicable to the specific intersection. For example, Section 1 contains 22 intersections which are labeled “A” through “V.” Intersection “A”, at S Whitley Dr and S Kansas Ave, is a T-intersection; therefore it has two corners which are numbered “1” and “2”.

Analysis of Pedestrian Ramps

In total, 248 pedestrian ramps were surveyed for this inventory. Measurements were taken at each location and the overall compliance of the ramp evaluated. Each ramp was given a rating from 1 to 3, as follows:

- 1 = the ramp meets current ADA criteria
- 2 = the ramp does not meet criteria and the existing ramp needs to be modified
- 3 = the ramp does not meet ADA criteria and needs to be completely reconstructed

The pedestrian ramp rating is determined by the quantity and type of work that needs to be completed in order for the ramp to meet current ADA standards. A ramp given a rating of 1 meets the maximum slope requirements, minimum width constraints for the ramp throat and landing, has an acceptable traversable path and drop-off depth at the flow line, and includes truncated domes. A rating of 2 indicates that the work needed does not require the ramp to be fully reconstructed. Possible improvements could include adding truncated domes, increasing the landing size, installing a traversable pathway around the ramp, and/or grinding the gutter pan to reduce the drop-off depth at the flow line. Ramps given a rating of 3 must be completely reconstructed to meet ADA standards. Ramps with this rating may have an unacceptable cross slope, running slope, and/or a narrow throat width.

For example, at the T-intersection of S Whitley Drive and S Kansas Ave, each of the 2 corners contains a single pedestrian ramp. Data collected at the ramps indicate that each needs to be completely reconstructed to meet current ADA criteria. Therefore, each receives a rating of 3. Pedestrian ramp information can be found in Appendix B, in the “Existing Conditions – Pedestrian Ramps” table. Table 1 provides an illustration of this example.

| EXISTING CONDITIONS - PEDESTRIAN RAMPS |              |          |                  |     |                      |              |   |
|--|--------------|----------|------------------|-----|----------------------|--------------|---|
| Section                                | Intersection | Corner # | Pedestrian Ramp? | GPS | Location             | Rating (1-3) | Notes   |
| 1                                      | A            | 1        | Yes              | 357 | S Whitley & S Kansas | 3            | No Truncated Domes, throat narrow at landing, narrow landing width, landing slope over 2% |
| 1                                      | A            | 2        | Yes              | 358 | S Whitley & S Kansas | 3            | No Truncated Domes, narrow landing width, landing slope over 2%                           |
| 1                                      | A            | 3        | N/A              |     |                      |              |   |
| 1                                      | A            | 4        | N/A              |     |                      |              |   |

Table 1: Example of the “Existing Conditions – Pedestrian Ramps” table in Appendix B

Some ramps that have a rating of 3 meet all of the ADA criteria except the cross slope requirement. A ramp that does not meet the threshold for cross slope generally needs full reconstruction to become compliant. In this situation, however, it is important to take into consideration the geometry of the adjacent road. If the adjacent road has a running slope that is greater than 2% it is very difficult to meet the 2% maximum cross slope threshold for pedestrian ramp landings and cross slope. Ramps with this condition are highlighted in yellow in the “Existing Conditions – Pedestrian Ramps” table in Appendix B.

The information provided in the “Existing Conditions – Pedestrian Ramps” table in Appendix B includes:

- Section: Refers to the Section Map where the street is located
- Intersection: Refers to specific intersection within the Section
- Corner #: Number given to each street corner
- Pedestrian Ramp:
  - “Yes” – There is an existing pedestrian ramp
  - “No” – There is not an existing pedestrian ramp and a ramp is needed
  - “N/A” – There is not an existing pedestrian ramp and ramp is unnecessary
- GPS: Ramp Location, which can be viewed on the GPS map in Appendix D
- Location: Name of the cross streets at the intersection
- Rating: Rating (1, 2, or 3) given to ramp based on the quantity and type of work needed for the ramp to be compliant
- Notes: Additional ramp information and concerns

#### Analysis of Sidewalks and Driveway Approaches

The “Existing Conditions – Sidewalks” table in Appendix C identifies the state of the existing sidewalks throughout the City of Fruitland. This table displays the section of the City in which the street being evaluated is located, the street name, and the cross streets. Each road has 2 sides, “A” and “B”. The table identifies whether or not the sidewalk is compliant with ADA standards. In the compliance column, “Yes” and “No” signify that the sidewalk does or does not meet ADA standards, respectively. “N/A” signifies that no sidewalk is present.

To determine whether the existing sidewalk is compliant, it was evaluated on the following criteria:

1. Minimum width of 4’
2. Continuous along street, no major cracks, obstructions, faulting or other discontinuities

A minimum width of 4’ is required to supply sufficient room for pedestrians to travel from one location to another. Major cracks, obstructions, and discontinuities were noted during inspection.

For example, the first row in the “Existing Conditions – Sidewalks” table indicates that the section of road being evaluated is in Section 1, which can be found on the Section 1 map in Appendix A. The street is S Kansas Ave located between S Whitley Dr and Fruitland High School. There are two sides of the street, labeled “A” and “B”, and each of these sides contains sidewalk that is non-compliant because of driveway approaches that do not meet ADA standards. Table 2 displays a portion of the “Existing Conditions – Sidewalks” table found in Appendix C.

| EXISTING CONDITIONS - SIDEWALKS |          |           |                       |      |            |  |
|---------------------------------|----------|-----------|-----------------------|------|------------|--|
| Section                         | Street   | From      | To                    | Side | Compliant? | Notes  |
| 1                               | S Kansas | S Whitley | Fruitland High School | A    | No         | Sidewalk in good condition, but contains non-compliant driveway approaches |
|                                 |          |           |                       | B    | No         | Sidewalk in good condition, but contains non-compliant driveway approaches |

Table 2: Example of the “Existing Conditions – Sidewalks” table that can be found in Appendix C

The information provided in the “Existing Conditions – Sidewalks” table that can be found in Appendix C includes:

- Section: Refers to what Section Map this street is located on
- Street: Name of the street surveyed
- From: Starting location
- To: Ending location
- Side: What side of the street being considered
  - A: Northernmost or Easternmost street side (depending on road orientation)
  - B: Southernmost or Westernmost street side (depending on road orientation)
- Compliant?:
  - “Yes” – Sidewalk exists and is compliant
  - “No” – Sidewalk is not compliant
  - “N/A” – No sidewalk exists
- Notes: Extra information

### Cost Estimate

Cost estimates for typical ADA improvements were developed for typical pedestrian ramps; 16-ft wide and 20-ft wide vehicular driveway approaches; and for 5-ft wide sidewalks. Cost estimates for these facilities were made in accordance with Idaho Standards for Public Works Construction (ISPWC) standard designs and include labor and materials required for excavation, preparation of aggregate base, construction of concrete flatwork (including 6” vertical curb and gutter) and minor landscape repair. Costs associated with mobilization, traffic control, stormwater pollution prevention or utility relocations were not included in the costs, but should be considered when constructing ADA facilities. Costs for the typical ADA improvements were then applied citywide in order to identify the overall cost to improve pedestrian facilities throughout Fruitland. Cost information is discussed later in this report.

### **Americans with Disabilities Act**

The Americans with Disabilities Act ensures that people with disabilities have access to State and local government facilities, places of public accommodation, and commercial facilities. Various criteria must be met in order for a pedestrian ramp, sidewalk or vehicular driveway approach across a sidewalk to be compliant with ADA standards. The minimum criteria for public rights-of-way related to pedestrian facilities are as follows:

1. Pedestrian ramp
  - a. Throat width at least 4' wide
  - b. Running slope no greater than 8.3%
  - c. Cross slope no greater than 2.0%<sup>1</sup>
2. Landing (may be located at top and/or bottom of pedestrian ramp)
  - a. Minimum dimensions: 4'x4'
  - b. Slope no greater than 2.0%
3. Detectable warning surface
  - a. Truncated domes present
4. Traversable path around pedestrian ramp present
  - a. Minimum 4' wide
5. Minimal Drop-off depth at flow line
  - a. Flush transition from curb ramp to gutter
  - b. Adjacent counterslopes in line of travel should not exceed 5%
6. Sidewalks
  - a. Minimum width of 4'
  - b. Cross slope no greater than 2.0%
  - c. Running slope no greater than 5.0%<sup>1</sup>
7. Driveway Approaches
  - a. Traversable path or compliant ramp
    - i. Traversable path at least 4' wide
    - ii. Ramp at least 4' wide
    - iii. Ramp cross slope no greater than 2.0%
    - iv. Ramp running slope no greater than 8.3%

The purpose of a 4' wide ramp, landing and traversable path are to assure sufficient room for a pedestrian to travel from place to place around a pedestrian ramp. Limitations on the maximum running and cross slopes ensure an accessible path. According to ADA requirements, detectable warning surfaces are required where curb ramps, blended transitions, or landings provide a pedestrian connection to the street. Truncated domes are to be placed at each pedestrian approach as a detectable warning surface. Their color and texture allow the disabled to become aware of an existing approach and the orientation provides direction of travel.

Complete ADA design guidelines can be found at the following website: <http://www.access-board.gov/prowac/index.htm>. The "Accessible Rights-of-Way: A Design Guide" provides useful information related to ADA standards for construction within public rights-of-way. Part I discusses regulatory requirements for accessible public rights-of-way and Part II discusses best practices in accessible rights-of-way design and construction. Part II, Chapter 3, provides information that was used to evaluate the minimum requirements for pedestrian ramps, sidewalks and driveway approaches in this analysis.

---

<sup>1</sup> Allowance may be made when slope of adjacent roadway is greater than 5.0%

## Existing Conditions

This section provides basic statistics relating to existing pedestrian ramps, sidewalks and driveway approaches surveyed within Fruitland. Information on common field observations is also presented.

### Pedestrian Ramps

Of the 248 existing pedestrian ramps encountered within the City of Fruitland, 65 (approximately 26%) are ADA compliant and 183 (approximately 74%) are non-compliant. In addition, there are 91 street corners where pedestrian ramps do not exist but may be needed. Many of the pedestrian ramps observed throughout Fruitland meet slope and throat width requirements. However, truncated domes, an appropriately sized landing at the top of the pedestrian ramp, and an acceptable traversable path around the pedestrian ramp are often missing. Figure 1 represents a typical pedestrian ramp found in Fruitland. This ramp has a rating of 3 because the ramp throat width is less than 48 inches, it has a narrow landing and traversable path, and the ramp does not have truncated domes.



Figure 1: Example of a non-compliant (Rating = 3) pedestrian ramp at the intersection of SW 8<sup>th</sup> and S Washington Ave. Location: Section 1-H(2).

Along the eastern side of Whitley Drive/US 95, most of the sidewalk consists of an asphalt pathway with concrete pedestrian ramps located at street corners. These ramps are not ADA compliant because they fail to meet minimum criteria. A majority of these ramps have poor drop-off depths at the flow line, steep cross slopes and running slopes, no truncated domes,

inadequate landings and no traversable pathways. Figure 2 provides an example of the pedestrian ramps and sidewalk located on Whitley Drive/US-95. This ramp has been given a rating of 3.



Figure 2: Example of a non-compliant (Rating = 3) pedestrian ramp at the intersection of S Whitley Dr and SW 4<sup>th</sup> St. Location: Section 2-II(1).

Other examples of ramps with a number 3 rating include:

- Intersection at S Kansas Ave and SW 3<sup>rd</sup> St
  - Location: 2-Z(1), 2-Z(2)
  - Problems: No truncated domes, steep running slope, no landing, no traversable path, poor drop-off depth at the flow line
- Intersection at N Pennsylvania and Oak Ave
  - Location: 4-O(1)
  - Problems: Narrow throat, narrow landing and traversable path, steep landing slope
- Intersection at N Pennsylvania Ave and NE 14<sup>th</sup> St
  - Location: 4-BB(2)
  - Problems: No truncated domes, steep running slope, narrow landing and traversable path
- Intersection at Golden Ave and Braeburn St
  - Location: 5-U(1), 5-U(2)
  - Problems: No truncated domes, narrow throat, steep running slope, no landing or traversable path

A majority of the pedestrian ramps that have a rating of 2 have a narrow landing, narrow traversable pathway, and no truncated domes. Figure 3 shows the typical layout of ramps that have received a rating of 2.



Figure 3: Example of non-compliant (Rating = 2) pedestrian ramp at the intersection of Quail Cove Dr and NW 3<sup>rd</sup> St. Location: Section 3-I(3).

Examples of other ramps that have received a rating of 2 include:

- Intersection at S Meadows Ave and SW 8<sup>th</sup> St (Neighborhood)
  - Location: 1-L(1), 1-L(2)
  - Problems: No truncated domes, small landing, narrow traversable path
- Intersection at S Colorado Ave and SW 3<sup>rd</sup> St (Park)
  - Location: 2-FF(1)
  - Problem: No truncated domes
- Intersection at Quail Cove Dr and NW 3<sup>rd</sup> St (Neighborhood)
  - Locations: 3-I(1), 3-I(2), 3-I(3), 3-I(4)
  - Problems: No truncated domes, small landing, narrow traversable path
- Intersection at N Vermont Ave and NW 12<sup>th</sup> St (Businesses)
  - Location 4-E(1)
  - Problem: Steep drop-off at flow line

Figures 4 and 5 illustrate two different existing pedestrian ramps that were observed in the City of Fruitland and are compliant with ADA criteria.



Figure 4: Existing pedestrian ramp (Rating = 1) on S. Pennsylvania Ave and SE 5<sup>th</sup> St.  
Location: Section 2-I(2)



Figure 5: Existing pedestrian ramp (Rating = 1) at the intersection of Syringa Springs and Magnolia. Location: Section 3-GG(2)

A complete list of the pedestrian ramps that were surveyed throughout Fruitland is located in Appendix B, under the “Existing Conditions – Pedestrian Ramps” table.

## Sidewalks

Throughout the City of Fruitland, 21.3 miles of roadway was surveyed. Of that, 7.8 miles (36%) of roadway exhibit pedestrian facilities that are compliant with ADA standards, 7.6 miles (36%) of roadway are non-compliant, and there are 5.9 miles (28%) where no sidewalk currently exists on either side of the street.

Sidewalk is considered necessary in areas where crosswalks are present or in areas with regular pedestrian traffic. The photo in Figure 6 was taken at SW 4<sup>th</sup> St and N Pennsylvania Ave. At this location, sidewalk is not present but needed along SW 4<sup>th</sup> St since a crosswalk exists and the area is subject to regular pedestrian usage. This section of 4<sup>th</sup> St makes up part of the 7.6 miles of roadway that are non-compliant and need sidewalk improvements..



Figure 6: Intersection at SW 4<sup>th</sup> and N Pennsylvania Ave. No sidewalk present, but needed.  
Location: Section 2-J(1).

Figure 7, below, gives an example of discontinuous sidewalk. This was prevalent in the downtown area of Fruitland, such as along SW 3<sup>rd</sup> St and SW 4<sup>th</sup> St.



Figure 7: Example of discontinuous sidewalk along SW 3<sup>rd</sup> St. Location: Section 2-GG.

### **Driveway Approaches**

Driveway approaches must have a traversable pathway around the approach or acceptable ramps that meet ADA criteria for pedestrian ramps. Various neighborhoods in the City of Fruitland do not have driveway approaches that are ADA-compliant. In addition, many commercial driveway approaches are non-compliant. An example of a commonly observed non-compliant driveway

approach can be seen in Figure 8. In this photo, there is no traversable pathway leading around the driveway approach. Also, the transition from the sidewalk to the approach is discontinuous.



Figure 8: Example of a non-compliant driveway approach along NE 12<sup>th</sup> St.  
Location: Section 4

Areas in Fruitland where approaches are non-compliant with ADA standards are listed below. A complete list can be found in Appendix C.

- Allen Ave (Section 5)
- Neighborhood between Applewood Ave and Golden Ave, north of NW 24<sup>th</sup> St (Section 5)
- NE 12<sup>th</sup> St (Section 4)
- Tara Ct (Section 4)
- Wild Rose Ct (Section 3)
- S Kansas Ave (Section 1)
- N Vermont Ave (Section 4)
- N Pennsylvania Ave (Section 4)

Driveway approaches with 3” rolled curb do not require modifications to become ADA compliant unless the sidewalk has discontinuities, obstructions or doesn’t meet the minimum width and maximum slope requirements. An example of this type of compliant driveway approach is shown in Figure 9.



Figure 9: Example of compliant driveway approaches along S Whitetail Ave  
Location: Section 1

## Recommendations

This section presents recommendations for bringing City of Fruitland facilities into compliance with ADA standards.

### General

#### Provide Pedestrian Connectivity

Designing pedestrian facilities that comply with ADA standards ensures access for people with disabilities. The minimum criteria set by ADA covers basic pedestrian needs and provides a safer environment for pedestrians.

In areas of high pedestrian usage, it is important to provide facilities for people to walk from location to location. The goal is to provide a continuous, accessible route for pedestrians of all abilities to use safely. Areas that often have high pedestrian traffic include: schools, churches,

community centers, shopping centers, large employers, parks or other recreational areas, hospitals and libraries. In general, areas where these types of facilities are present should exhibit ADA-compliant routes on at least one side of the street, with compliant routes on both sides preferred. The sidewalk should provide a continuous, accessible route for pedestrians trying to reach their destination. Intersections along these routes should exhibit ADA-compliant pedestrian ramps at all corners with properly signed and striped crosswalks between. Driveways and approaches should also exhibit ADA-compliant geometry and features.

Examples of areas within the City of Fruitland where ADA-compliant routes should be provided:

- SW 3<sup>rd</sup> St – access to small businesses and shops, Post Office, park, community center
- SW 4<sup>th</sup> St – church access
- S Iowa Ave – access to Fruitland High School, Fruitland Intermediate School and businesses
- S Nebraska Ave – access to businesses
- S Kansas Ave – access to businesses, Fruitland High School
- N and S Pennsylvania Ave – access to churches, park, and numerous residential areas

Figure 10 displays an area in Fruitland near two schools where there is a non-compliant route. This intersection has a crosswalk, but no pedestrian ramp connecting the sidewalk to the crosswalk. Also, the driveways along this street are non-compliant, as they do not possess a traversable pathway around the approaches. Obstructions such as telephone poles and mailboxes within the sidewalk are also present along this street, as shown in the photo.

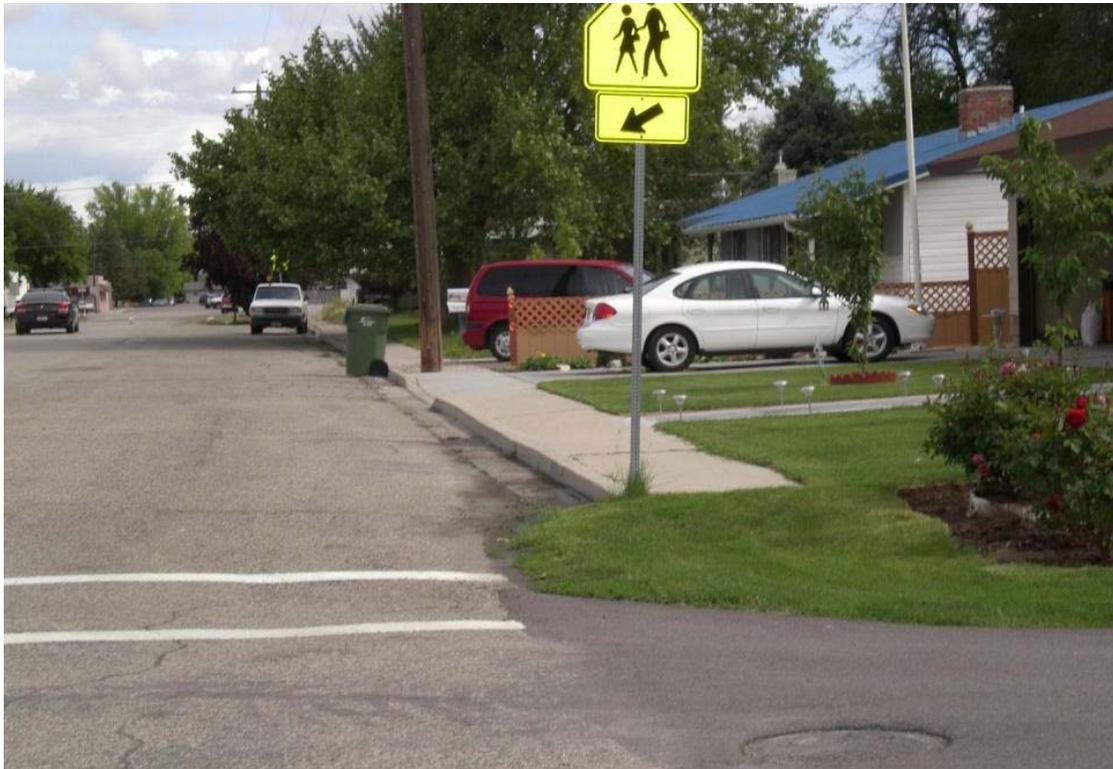


Figure 10: Example of non-compliant sidewalk. Location: Section 2-P(2).



## **Pedestrian Ramps**

Typical corrective action for pedestrian ramp locations can be found in several resource documents. Two examples are the ISPWC and the Idaho Transportation Department (ITD) Standard Drawings. Section 700 (Concrete) of the ISPWC outlines several types of pedestrian ramps which meet or exceed the minimum tolerances for ADA compliance. Additional design information may be obtained from the City of Fruitland's supplement to the ISPWC. ITD Standard Drawing H-2-B identifies typical pedestrian ramp facilities to be constructed on State Highway facilities. Developers should be held responsible for meeting or exceeding ADA requirements and should follow nationally accepted standards for pedestrian ramp construction.

Out of the 248 pedestrian ramps observed throughout Fruitland, 65 have a rank of 1. These 65 meet ADA Standards and do not need any improvements. It is recommended that no improvements be made to these 65 pedestrian ramps.

Conversely, 59 pedestrian ramps have been given a rating of 2. These ramps need minor improvements that may include: the addition of truncated domes, installation of a traversable pathway at the top of the existing ramp, a 4'x4' landing, and/or grinding of the drop-off discontinuity at the flow line. None of the aforementioned improvements require full reconstruction of the pedestrian ramp. It is recommended that site specific modifications be made to these 59 existing ramps in order to bring them into compliance. On a site-by-site basis, costs and the overall feasibility of making these improvements should be evaluated to determine whether full reconstruction is cost-effective compared to retro-fitting existing facilities.

Throughout Fruitland, 124 pedestrian ramps have a rating of 3 and need to be fully reconstructed because the adjustments required to bring the ramp into compliance cannot be made cost-effectively without building an entirely new ramp. It is recommended that the City work towards full reconstruction of each of these 124 pedestrian ramps.

As noted before, it is important to consider the adjacent roadway characteristics when determining the pedestrian ramp's ADA-compliance ranking. Certain attributes of the adjacent roadway (i.e. longitudinal slope) will affect the geometry of pedestrian ramps and may make it infeasible to achieve ADA compliance in an economically responsible manner. Within the constraints presented by the roadway geometry, pedestrian ramps should be constructed or reconstructed to meet ADA requirements as close as possible. For example, if the longitudinal slope of a roadway is 7%, it may be infeasible to retrofit existing pedestrian ramps so that the throat cross slope is 2% or less without major reconstruction to the entire roadway. Any pedestrian ramp that is replaced along this section of roadway should exhibit as many ADA compliant attributes as possible, but full compliance may not be feasible within reasonable financial limits.

## **Sidewalks**

All sidewalks need to maintain a minimum traversable path of 4' and have a maximum cross slope of 2.0%. In certain instances, the clear width can be reduced to 32", over a maximum

length of 24". This is illustrated in Figure 12, where the sidewalk has been narrowed to 32" because of the protruding obstruction.

If the minimum ADA criteria are not met, then the sidewalk should be reconstructed. In some instances where minor discontinuities are present, individual slabs should be replaced. In areas where bumps or faults exist, grinding should be performed to smooth out the sidewalk surface.

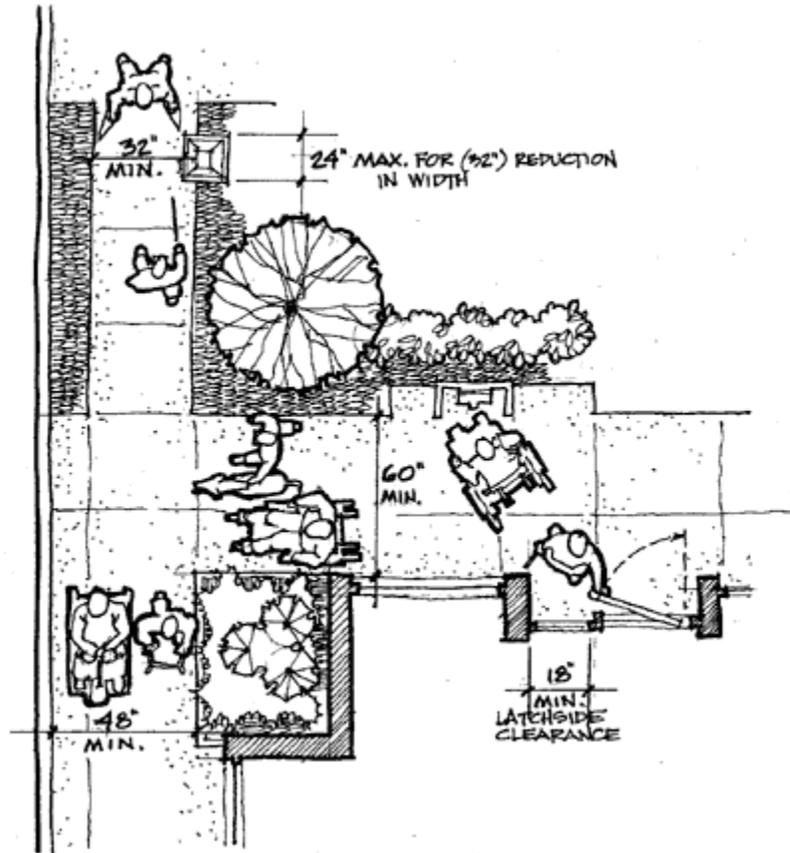


Figure 12: Minimum accessible sidewalk widths

Figure Obtained From: <http://www.access-board.gov/prowac/guide/figure1.htm>

## Driveway Approaches

A traversable pathway around the back of the driveway approach is necessary if the driveway slopes towards the street. Many existing driveways in Fruitland are not compliant because they do not exhibit the traversable pathway behind the approach. These driveway approaches should be retro-fitted with the traversable pathway or reconstructed altogether. An alternative driveway approach is as shown in Figure 13. This type of driveway eliminates the need for a traversable pathway behind the driveway approach because the driveway, itself, provides a traversable path.

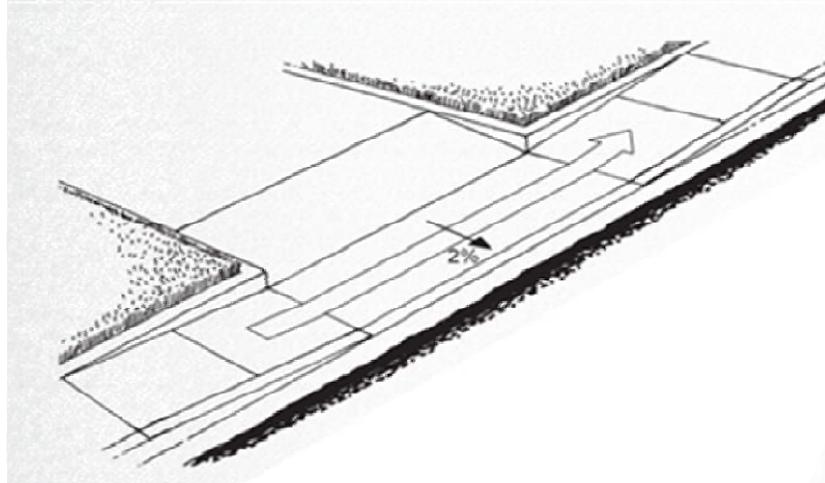


Figure 13: Drop-down style driveway approach

Figure Obtained From: <http://www.fhwa.dot.gov/download/hep/environment/06chapter5.pdf>

Some existing commercial driveway approaches exhibit pedestrian ramps, similar to a roadway intersection. At these locations, the ramps on both sides of the driveway must meet all ADA criteria set forth for standard pedestrian ramps and should be retro-fitted or reconstructed so as to meet the criteria.

In some cases, rolled curb is present and no physical driveway feature exists. Vehicles are permitted to drive over the rolled curb and sidewalk. Therefore, the sidewalk is continuous and traversable at these locations and no alternative pathway around an approach is required. Driveway approaches exhibiting 3" rolled curb do not require modifications to become ADA compliant unless the associated sidewalk has discontinuities, obstructions or doesn't meet the minimum width and maximum slope requirements. Therefore, no improvements are recommended for driveway locations exhibiting 3" rolled curb as long as the sidewalk itself is compliant.

## Cost of Improvements

Several components must be considered when evaluating the cost of constructing new or retrofitting existing pedestrian facilities to bring them into compliance with ADA standards. The minimal amount of raw materials (e.g. concrete, aggregate base, etc.) required to bring existing pedestrian facilities into compliance with ADA standards constitutes a relatively small portion of the overall cost. The majority of the cost is realized through mobilization of contractors for small amount of work, relocation of utilities when required, setting up and maintaining traffic control, and distributing the small amount of raw materials to unique project sites located throughout the city. The remainder of this section discusses costs associated with constructing new pedestrian ramps, new driveway approaches and sidewalk that meet ADA standards.

## **Pedestrian Ramp Improvements**

A cost breakdown was developed for a typical urban Type “A” (ISPWC) pedestrian ramp and is included in Appendix F. The materials and labor required to construct a single Type “A” ADA compliant pedestrian ramp is estimated at \$1463 per ramp. All pedestrian ramps that have received a rating of 3 and locations where ramps are currently non-existent but needed, are estimated at this cost. This includes excavation of existing soil or old ramp material, construction of aggregate base, construction of concrete pedestrian ramp and repair of landscaping around the new pedestrian ramp. This cost does not include construction traffic control, erosion & sediment control, utility relocation or site-specific incidental costs.

There are 124 existing pedestrian ramps that have a rating of 3. At \$1463 per ramp, the total cost to construct these ramps is \$181,406. In addition, there are 91 pedestrian ramps that need to be constructed in areas where no pedestrian ramp currently exists. The unit replacement of these ramps is also \$1463, summing to \$133,129. Therefore, the total number of ramps that require full construction is 215 and the total cost for these ramps is \$314,534.

There are 59 currently existing pedestrian ramps that have been given a rating of 2. To make these ramps ADA compliant, full reconstruction is not necessary. Each location would need a site-specific cost estimate but an assumption has been made that the cost of retro-fitting an existing ramp is 60% of the cost of replacing a ramp altogether. Therefore, the cost to update one pedestrian ramp with a rating of 2 has been estimated at \$878. This makes the total cost for these types of ramps \$51,788. A summary of these costs is provided in Table 3, at the end of the “Costs of Improvements” section.

## **Sidewalk Improvements**

A cost breakdown was developed for replacement of a typical 5-ft x 5-ft concrete slab as per ISPWC SD-709 and is included in Appendix F. The materials and labor required to construct a single 5-ft x 5-ft ADA compliant slab of sidewalk is estimated at \$98. This includes excavation of existing soil or old slab material, construction of aggregate base, construction of concrete slab and repair of landscaping around the new sidewalk. This cost does not include construction traffic control, erosion & sediment control, utility relocation or site-specific incidental costs. The cost also does not include construction of sidewalk on streets that currently have no sidewalk facilities, but allows for the construction of short segments of sidewalk where minor discontinuities occur. It should also be noted that proposed sidewalk repair locations may also overlap locations where future roadway projects are planned (e.g. SW 3<sup>rd</sup> Street). These projects may eliminate the need to address pedestrian facility improvements where overlaps occur. The length of existing sidewalk needing replacement or construction to achieve ADA compliance is estimated at 3630 feet. Therefore, an equivalent of 726 concrete slabs need replacement. The cost estimate for this construction is estimated at \$71,435.

## Driveway Approach Improvements

A cost breakdown was developed for two typical urban vehicle approaches as per ISPWC (SD-710A) and is included in Appendix F. Two different throat widths were evaluated: 16-ft and 20-ft. The materials and labor required to construct a single ADA compliant driveway for the two different throat widths is estimated at \$2626 and \$3014, respectively. This includes excavation of existing soil or old driveway material, construction of aggregate base, and repair of landscaping around the new driveway. This cost does not include construction traffic control, erosion & sediment control, utility relocation or site-specific incidental costs. There are estimated to be 237 non-compliant driveway approaches that are 16-ft long, totaling to \$622,393. There are an estimated 55 driveway approaches that are 20-ft in length, totaling to \$165,783.

Table 3 provides a summary of the predicted costs. The total cost of all improvements previously discussed is estimated at \$1,174,145.

| COST SUMMARY                             |                       |                |               |
|--|-----------------------|----------------|---------------|
| Item                                     | Unit Replacement Cost | Total Quantity | Total Cost    |
| Pedestrian Ramp (R=2)                    | \$ 877.77             | 59             | \$ 51,788.46  |
| Pedestrian Ramp (R=3)                    | \$ 1,462.95           | 124            | \$ 181,405.91 |
| Pedestrian Ramp (Currently non-existent) | \$ 1,462.95           | 91             | \$ 133,128.53 |
| 16' Driveway Approach                    | \$ 2,626.13           | 237            | \$ 622,392.96 |
| 20' Driveway Approach                    | \$ 3,014.23           | 55             | \$ 165,782.62 |
| 5'x5' Sidewalk Slab                      | \$ 98.40              | 726            | \$ 71,434.81  |
|  |                       | <b>Sum:</b>    | \$ 1,174,145  |

Table 3: Summary of Predicted Improvement Costs

## Phasing of Improvements

Whereas general roadway improvement projects identified in the Fruitland Master Transportation Plan (2010) will include provisions to construct new or improve existing pedestrian facilities, not all roads within Fruitland are slated for improvements in the foreseeable future. It is therefore recommended that the City of Fruitland to engage in a separate pedestrian improvements program.

Correcting all ADA deficiencies throughout town or adding pedestrian facilities where there currently are none all at one time would be financially infeasible. It is recommended that a phased plan be adopted to address the deficiencies in a timely manner. The City of Fruitland should focus effort on constructing or reconstructing pedestrian facilities in high pedestrian use areas and areas where mobility of the disabled community is critical. Additionally, the City should address existing non-compliant facilities in a manner that replaces the older, deteriorated

non-compliant facilities before replacing the recently constructed facilities that are in good condition and present only minor compliance issues. Project locations should be identified one or two years in advance of construction so that resources can be effectively allocated.

The City of Fruitland should allocate a percentage of the annual roadway improvements budget to improving pedestrian facilities. With hundreds of locations identified as either non-compliant or lacking facilities altogether, a reasonable goal would be to repair or replace 2-5% of the deficient facilities annually. At the low end, completing 2% of the proposed improvements per year would constitute repair of approximately 6 non-compliant pedestrian ramps, 6 non-compliant driveways, and approximately 75 linear feet of non-compliant sidewalk. Alternatively, completing 5% of the improvements per year would constitute repair of approximately 14 non-compliant pedestrian ramps, 15 non-compliant driveways, and approximately 185 linear feet of non-compliant sidewalk.

If grants or additional City funds become available, additional effort should be applied to repairing non-compliant pedestrian facilities.

## **Summary**

Through data collection and analysis, it has been concluded that there are locations within the City of Fruitland that are in need of pedestrian facility improvements to meet ADA standards. Nearly 75% of existing pedestrian ramps are non-compliant and require either partial or full-reconstruction to be classified as ADA-compliant with current standards. There are also locations, as provided in the “Existing Conditions – Pedestrian Ramps” Table, where pedestrian ramps are non-existent but needed in order to provide proper access for pedestrians. At 274 locations, pedestrian ramps need to be modified, fully-reconstructed, or added where no ramp currently exists.

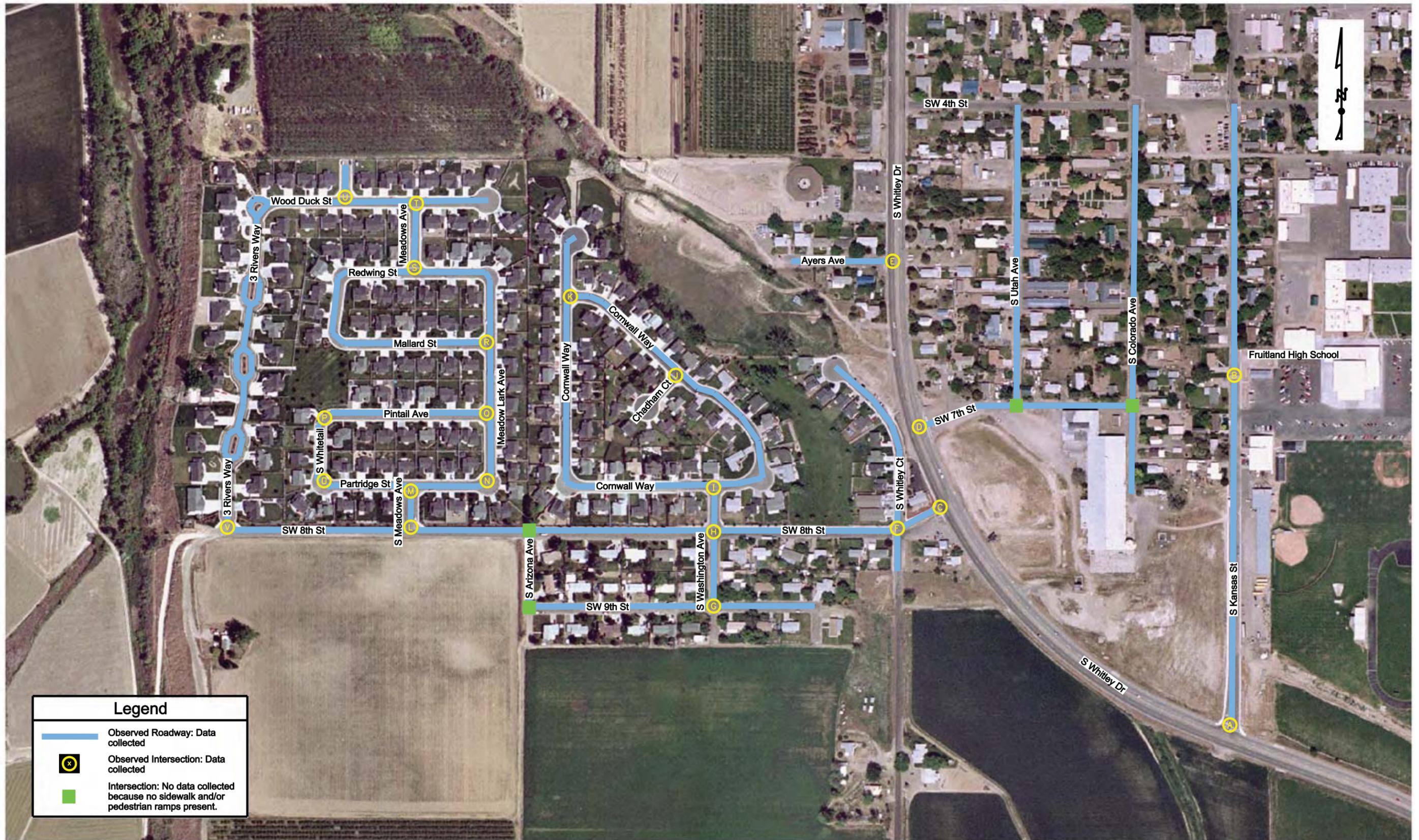
Along with installing and updating pedestrian ramps, it is suggested that Fruitland improve sidewalks and driveway approaches at certain locations, as provided in the “Existing Conditions – Sidewalk” table. Out of the 21.3 miles of roadway surveyed, approximately 8 miles have existing facilities that need improvement. Also, nearly 6 miles of roadway does not currently exhibit sidewalk and—depending on the location—may need to have sidewalk installed in order to provide continuous ADA routes through the City. Typically, sidewalk is constructed with roadway improvement projects that also include curb & gutter. Some sections of sidewalk are non-compliant due to sloped driveway approaches that inhibit access, whereas other areas have discontinuous sidewalk or sidewalk in poor condition. The estimated length of sidewalk to be replaced is 3,630 ft.

The estimated total cost of all improvements is \$1,174,145. Completion of the suggested improvements will update Fruitland’s pedestrian facilities so that they are in compliance with current ADA standards.

# **Appendix A:**

**Section Maps (1-5)**

**Observed Intersections & Roadway**



Section 1 Map: Observed Intersections & Roadway



| Legend  |  |
|---|--|
|  | Observed Roadway: Data collected   |
|  | Observed Intersection: Data collected  |
|  | Intersection: No data collected because no sidewalk and/or pedestrian ramps present. |

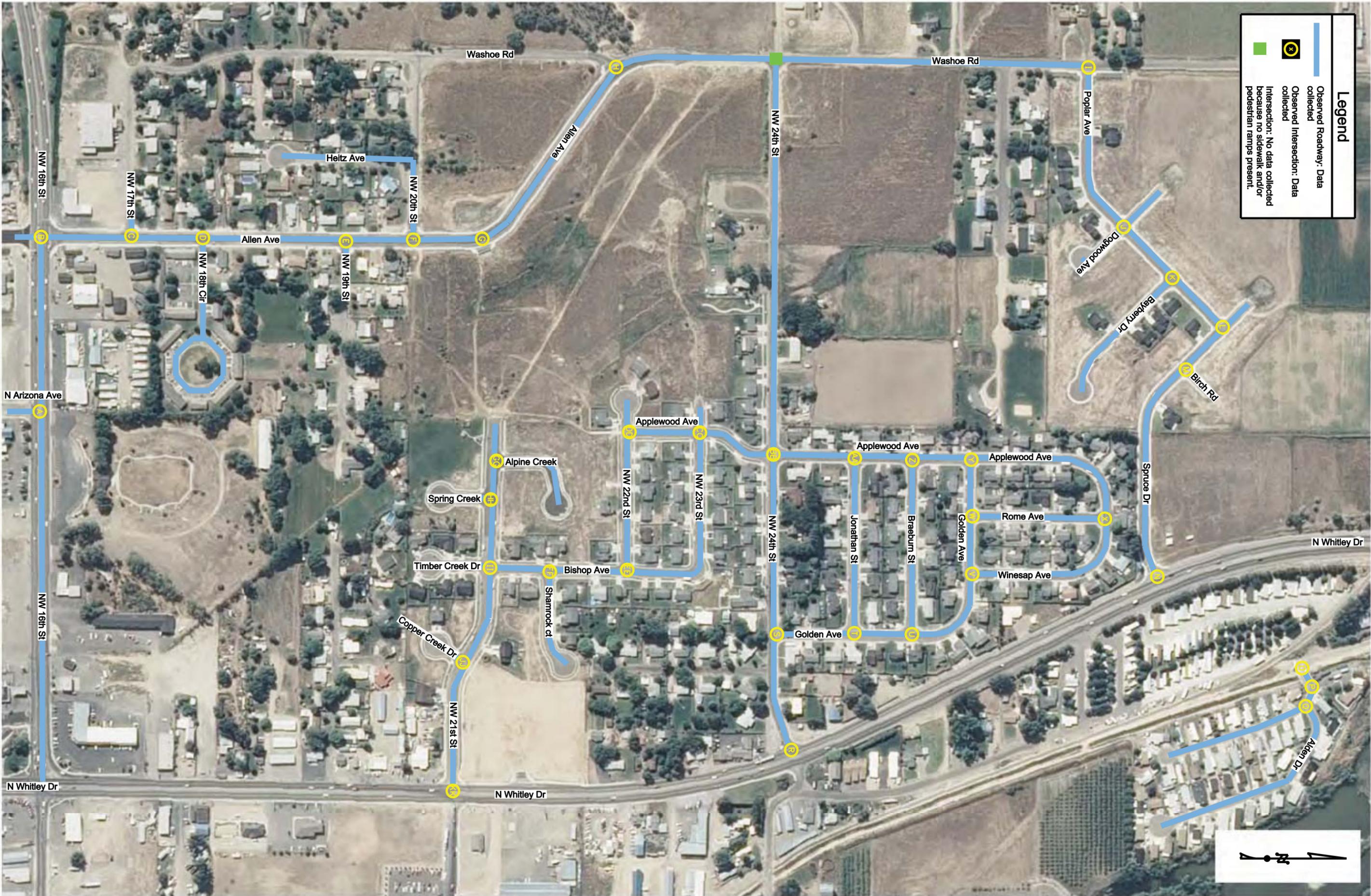
Section 2 Map: Observed Intersections & Approaches



Section 3 Map: Observed Intersections & Roadway

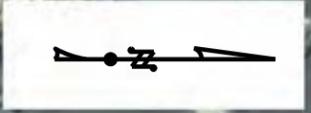


Section 4 Map: Observed Intersections & Roadway



**Legend**

- Observed Roadway: Data collected
- Observed Intersection: Data collected
- Intersection: No data collected because no sidewalk and/or pedestrian ramps present.



Section 5 Map: Observed Intersections & Roadway

# **Appendix B:**

## **Existing Conditions – Pedestrian Ramps Table**

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS | Location                    | Rating (1-3) | Notes   |
|---------|--------------|----------|------------------|-----|-----------------------------|--------------|---|
| 1       | A            | 1        | Yes              | 357 | S Whitley & S Kansas        | 3            | No Truncated Domes, throat narrow at landing, narrow landing width, landing slope over 2% |
| 1       | A            | 2        | Yes              | 358 | S Whitley & S Kansas        | 3            | No Truncated Domes, narrow landing width, landing slope over 2%                           |
| 1       | A            | 3        | N/A              |     |                             |              |   |
| 1       | A            | 4        | N/A              |     |                             |              |   |
| 1       | B            | 1        | No               | 579 | S Kansas Ave                |              | Sidewalk & crosswalk but no approach  |
| 1       | B            | 2        | No               | 580 | S Kansas Ave                |              | Crosswalk but no sidewalk or approach   |
| 1       | B            | 3        | N/A              |     |                             |              |   |
| 1       | B            | 4        | N/A              |     |                             |              |   |
| 1       | C            | 1        | Yes              | 190 | S Whitley & SW 8th          | 2            | No Truncated Domes, No landing, No traversable path                                       |
| 1       | C            | 2        | N/A              |     |                             |              |   |
| 1       | C            | 3        | N/A              |     |                             |              |   |
| 1       | C            | 4        | N/A              |     |                             |              |   |
| 1       | D            | 1        | Yes              | 224 | S Whitley & SW 7th          | 3            | No Truncated Domes, Landing too small, poor drop-off                                      |
| 1       | D            | 2        | Yes              | 225 | S Whitley & SW 7th          | 3            | No Truncated Domes, Landing too small, poor drop-off                                      |
| 1       | D            | 3        | N/A              |     |                             |              |   |
| 1       | D            | 4        | N/A              |     |                             |              |   |
| 1       | E            | 1        | Yes              | 360 | S Whitley & Ayers Ave       | 2            | No Truncated Domes, Landing too small   |
| 1       | E            | 2        | No               | 359 | S Whitley & Ayers Ave       |              |   |
| 1       | E            | 3        | N/A              |     |                             |              |   |
| 1       | E            | 4        | N/A              |     |                             |              |   |
| 1       | F            | 1        | Yes              | 186 | S Whitley Ct & SW 8th St    | 3            | No Truncated Domes, Landing too small, landing slope too large                            |
| 1       | F            | 2        | Yes              | 188 | S Whitley Ct & SW 8th St    | 3            | No Truncated Domes, cross slope too large, Landing too small, landing slope too large     |
| 1       | F            | 3        | No               |     | S Whitley Ct & SW 8th St    |              |   |
| 1       | F            | 4        | Yes              | 187 | S Whitley Ct & SW 8th St    | 3            | No Truncated Domes, No landing, No traversable path                                       |
| 1       | G            | 1        | N/A              | 354 | S Washington & SW 9th       |              | No Sidewalk   |
| 1       | G            | 2        | N/A              | 353 | S Washington & SW 9th       |              | No Sidewalk   |
| 1       | G            | 3        | N/A              | 356 | S Washington & SW 9th       |              | No Sidewalk   |
| 1       | G            | 4        | N/A              | 355 | S Washington & SW 9th       |              | No Sidewalk   |
| 1       | H            | 1        | Yes              | 195 | S Washington & SW 8th       | 3            | No Truncated Domes, Landing too small, Narrow Traversable Path                            |
| 1       | H            | 2        | Yes              | 194 | S Washington & SW 8th       | 3            | No Truncated Domes, Landing too small, Narrow Traversable Path                            |
| 1       | H            | 3        | N/A              |     |                             |              |   |
| 1       | H            | 4        | N/A              |     |                             |              |   |
| 1       | I            | 1        | Yes              | 196 | S Washington & Cornwall Way | 3            | No Truncated Domes, Narrow throat, Landing too small, Narrow traversable path             |
| 1       | I            | 2        | Yes              | 197 | S Washington & Cornwall Way | 3            | No Truncated Domes, Narrow throat, Landing too small, Narrow traversable path             |
| 1       | I            | 3        | N/A              |     |                             |              |   |
| 1       | I            | 4        | N/A              |     |                             |              |   |
| 1       | J            | 1        | Yes              | 199 | Cornwall Way & Chadham Crt  | 2            | No Truncated Domes, Landing too small, Narrow traversable path                            |
| 1       | J            | 2        | Yes              | 198 | Cornwall Way & Chadham Crt  | 3            | No Truncated Domes, Landing too small, Narrow traversable path                            |
| 1       | J            | 3        | N/A              |     |                             |              |   |
| 1       | J            | 4        | N/A              |     |                             |              |   |
| 1       | K            | 1        | Yes              | 200 | Cornwall Way                | 3            | No Truncated Domes, Narrow throat, Landing too small, Narrow traversable path             |
| 1       | K            | 2        | Yes              | 201 | Cornwall Way                | 2            | No Truncated Domes, Narrow throat, Landing too small, Narrow traversable path             |
| 1       | K            | 3        | N/A              |     |                             |              |   |
| 1       | K            | 4        | N/A              |     |                             |              |   |
| 1       | L            | 1        | Yes              | 221 | S Meadows Ave & SW 8th      | 2            | No Truncated Domes, Landing too small, landing slope too large, narrow traversable path   |
| 1       | L            | 2        | Yes              | 222 | S Meadows Ave & SW 8th      | 2            | No Truncated Domes, Landing too small, Narrow Traversable Path                            |
| 1       | L            | 3        | N/A              |     |                             |              |   |
| 1       | L            | 4        | N/A              |     |                             |              |   |

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS | Location                       | Rating (1-3) | Notes  |
|---------|--------------|----------|------------------|-----|--------------------------------|--------------|--|
| 1       | M            | 1        | Yes              | 581 | S Meadows Ave & Partridge St   | 3            | No truncated domes, narrow throat width, large cross slope & landing slope, small landing & narrow traversable path  |
| 1       | M            | 2        | Yes              | 582 | S Meadows Ave & Partridge St   | 3            | No truncated domes, narrow throat width, small landing & narrow traversable path                                     |
| 1       | M            | 3        | N/A              |     |                                |              |  |
| 1       | M            | 4        | N/A              |     |                                |              |  |
| 1       | N            | 1        | Yes              | 203 | Meadow Lark Ave & Partridge St | 3            | No Truncated Domes, Narrow throat, cross slope too large, Landing too small, narrow traversable path                 |
| 1       | N            | 2        | N/A              |     |                                |              |  |
| 1       | N            | 3        | N/A              |     |                                |              |  |
| 1       | N            | 4        | N/A              |     |                                |              |  |
| 1       | O            | 1        | Yes              | 208 | S Whitetail & Partridge St     | 3            | No Truncated Domes, Narrow throat, cross slope too large, Landing too small, narrow traversable path                 |
| 1       | O            | 2        | N/A              |     |                                |              |  |
| 1       | O            | 3        | N/A              |     |                                |              |  |
| 1       | O            | 4        | N/A              |     |                                |              |  |
| 1       | P            | 1        | Yes              | 206 | S Whitetail & Pintail St       | 3            | No Truncated Domes, Narrow throat, Landing too small, narrow traversable path  |
| 1       | P            | 2        | N/A              |     |                                |              |  |
| 1       | P            | 3        | N/A              |     |                                |              |  |
| 1       | P            | 4        | N/A              |     |                                |              |  |
| 1       | Q            | 1        | Yes              | 205 | Meadow Lark Ave & Pintail St   | 3            | No Truncated Domes, Narrow Throat, Landing too small, narrow traversable path  |
| 1       | Q            | 2        | Yes              | 204 | Meadow Lark Ave & Pintail St   | 2            | No Truncated Domes, Landing too small, narrow traversable path   |
| 1       | Q            | 3        | N/A              |     |                                |              |  |
| 1       | Q            | 4        | N/A              |     |                                |              |  |
| 1       | R            | 1        | Yes              | 210 | Meadow Lark Ave & Mallard      | 3            | No Truncated Domes, Narrow Throat, Landing too small, narrow traversable path  |
| 1       | R            | 2        | Yes              | 209 | Meadow Lark Ave & Mallard      | 3            | No Truncated Domes, Narrow Throat, Landing too small, narrow traversable path  |
| 1       | R            | 3        | N/A              |     |                                |              |  |
| 1       | R            | 4        | N/A              |     |                                |              |  |
| 1       | S            | 1        | Yes              | 211 | Meadows Ave & Redwing St       | 3            | No Truncated Domes, Narrow Throat, Landing too small, narrow traversable path  |
| 1       | S            | 2        | Yes              | 212 | Meadows Ave & Redwing St       | 3            | No Truncated Domes, Narrow Throat, cross slope too large, Landing too small, narrow traversable path                 |
| 1       | S            | 3        | N/A              |     |                                |              |  |
| 1       | S            | 4        | N/A              |     |                                |              |  |
| 1       | T            | 1        | Yes              | 216 | Meadows Ave & Wood Duck St     | 3            | No Truncated Domes, Narrow Throat, cross slope & landing slope too large, Landing too small, narrow traversable path |
| 1       | T            | 2        | Yes              | 215 | Meadows Ave & Wood Duck St     | 3            | No Truncated Domes, Narrow Throat, cross slope & landing slope too large, Landing too small, narrow traversable path |
| 1       | T            | 3        | N/A              |     |                                |              |  |
| 1       | T            | 4        | N/A              |     |                                |              |  |
| 1       | U            | 1        | Yes              | 218 | Wood Duck St                   | 3            | No Truncated Domes, Narrow Throat, Landing too small, narrow traversable path  |
| 1       | U            | 2        | Yes              | 217 | Wood Duck St                   | 3            | No Truncated Domes, Narrow Throat, Cross slope & landing slope too large, Landing too small, narrow traversable path |
| 1       | U            | 3        | N/A              |     |                                |              |  |
| 1       | U            | 4        | N/A              |     |                                |              |  |
| 1       | V            | 1        | Yes              | 219 | 3 Rivers Way & SW 8th          | 2            | No Truncated Domes, Landing too small, narrow traversable path   |
| 1       | V            | 2        | Yes              | 220 | 3 Rivers Way & SW 8th          | 2            | No Truncated Domes, Landing too small, landing slope too large, narrow traversable path                              |
| 1       | V            | 3        | N/A              |     |                                |              |  |
| 1       | V            | 4        | N/A              |     |                                |              |  |

|   |   |   |     |     |                            |   |  |
|---|---|---|-----|-----|----------------------------|---|--|
| 2 | A | 1 | Yes | 280 | US HWY 95 & S Pennsylvania | 3 | No Truncated Domes, Landing too small, Narrow traversable path, poor drop-off at flow line |
| 2 | A | 2 | Yes | 279 | US HWY 95 & S Pennsylvania | 3 | No Truncated Domes, Landing too small, Narrow traversable path, poor drop-off at flow line |
| 2 | A | 3 | N/A |     |                            |   |  |
| 2 | A | 4 | N/A |     |                            |   |  |
| 2 | B | 1 | No  | 278 | S Pennsylvania             |   | Crosswalk but no approach  |
| 2 | B | 2 | No  | 276 | S Pennsylvania             |   | Crosswalk but no approach  |
| 2 | B | 3 | No  | 277 | S Pennsylvania             |   | Crosswalk but no approach  |
| 2 | B | 4 | N/A |     |                            |   |  |

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS     | Location                | Rating (1-3) | Notes   |
|---------|--------------|----------|------------------|---------|-------------------------|--------------|---|
| 2       | C            | 1        | Yes              | 591     | S Pennsylvania          | 1            |   |
| 2       | C            | 2        | Yes              | 592     | S Pennsylvania          | 1            |   |
| 2       | C            | 3        | N/A              |         |                         |              |   |
| 2       | C            | 4        | N/A              |         |                         |              |   |
| 2       | D            | 1        | Yes              | 589     | S Pennsylvania          | 1            |   |
| 2       | D            | 2        | Yes              | 590     | S Pennsylvania          | 1            |   |
| 2       | D            | 3        | N/A              |         |                         |              |   |
| 2       | D            | 4        | N/A              |         |                         |              |   |
| 2       | E            | 1        | Yes              | 587     | S Pennsylvania          | 1            |   |
| 2       | E            | 2        | Yes              | 588     | S Pennsylvania          | 1            |   |
| 2       | E            | 3        | N/A              |         |                         |              |   |
| 2       | E            | 4        | N/A              |         |                         |              |   |
| 2       | F            | 1        | Yes              | 586     | S Pennsylvania          | 1            |   |
| 2       | F            | 2        | Yes              | 583/584 | S Pennsylvania          | 1            |   |
| 2       | F            | 3        | Yes              | 585     | S Pennsylvania          | 1            |   |
| 2       | F            | 4        | N/A              |         |                         |              |   |
| 2       | G            | 1        | Yes              | 260/261 | S Pennsylvania & SW 7th | 1            | 2 ped ramps at corner                                       |
| 2       | G            | 2        | Yes              | 263     | S Pennsylvania & SW 7th | 1            |   |
| 2       | G            | 3        | No               | 262     | S Pennsylvania & SW 7th |              | Sidewalk but no approach, crosswalk running N/S             |
| 2       | G            | 4        | N/A              |         |                         |              |   |
| 2       | H            | 1        | Yes              | 258     | S Pennsylvania & SE 6th | 1            | Crosswalk running N/S                                       |
| 2       | H            | 2        | Yes              | 259     | S Pennsylvania & SE 6th | 1            | Crosswalk running N/S                                       |
| 2       | H            | 3        | N/A              |         |                         |              |   |
| 2       | H            | 4        | N/A              |         |                         |              |   |
| 2       | I            | 1        | Yes              | 255     | S Pennsylvania & SE 5th | 1            | Crosswalk running E/W                                       |
| 2       | I            | 2        | Yes              | 256     | S Pennsylvania & SE 5th | 1            | Crosswalk running N/S & E/W                                 |
| 2       | I            | 3        | Yes              | 257     | S Pennsylvania & SE 5th | 1            | Crosswalk running N/S                                       |
| 2       | I            | 4        | N/A              |         |                         |              |   |
| 2       | J            | 1        | No               | 254     | S Pennsylvania & SW 4th |              | No Sidewalk or approach                                     |
| 2       | J            | 2        | Yes              | 253     | S Pennsylvania & SW 4th | 1            |   |
| 2       | J            | 3        | N/A              |         |                         |              |   |
| 2       | J            | 4        | N/A              |         |                         |              |   |
| 2       | K            | 1        | No               | 284     | S Minnesota & SW 3rd    |              | Sidewalk & crosswalk running N/S & E/W, no approach         |
| 2       | K            | 2        | No               | 283     | S Minnesota & SW 3rd    |              | No sidewalk, Crosswalk running E/W                          |
| 2       | K            | 3        | No               | 282     | S Minnesota & SW 3rd    |              | No sidewalk, Crosswalk running E/W                          |
| 2       | K            | 4        | No               | 281     | S Minnesota & SW 3rd    |              | Sidewalk & crosswalk running N/S & E/W, no approach         |
| 2       | L            | 1        | No               | 252     | S Minnesota & SW 4th    |              | Sidewalk  |
| 2       | L            | 2        | No               | 251     | S Minnesota & SW 4th    |              | Sidewalk, no curb/gutter                                    |
| 2       | L            | 3        | No               | 250     | S Minnesota & SW 4th    |              | Sidewalk running E/W, crosswalk running N/S, no curb/gutter |
| 2       | L            | 4        | No               | 249     | S Minnesota & SW 4th    |              | Sidewalk running E/W, crosswalk running N/S, no curb/gutter |
| 2       | M            | 1        | No               | 269     | S Minnesota & S 5th     |              | Sidewalk running N/S, Crosswalk running E/W                 |
| 2       | M            | 2        | No               | 268     | S Minnesota & S 5th     |              | Sidewalk running N/S, Crosswalk running E/W                 |
| 2       | M            | 3        | No               | 267     | S Minnesota & S 5th     |              | Sidewalk running N/S  |
| 2       | M            | 4        | N/A              | 270     | S Minnesota & S 5th     |              | No Sidewalk   |
| 2       | N            | 1        | No               | 265     | S Minnesota & SW 7th    |              | No Sidewalk, Crosswalk running N/S                          |
| 2       | N            | 2        | No               | 264     | S Minnesota & SW 7th    |              | No sidewalk   |
| 2       | N            | 3        | No               | 266     | S Minnesota & SW 7th    |              | No Sidewalk, Crosswalk running N/S                          |
| 2       | N            | 4        | N/A              |         |                         |              |   |
| 2       | O            | 1        | No               | 274     | S Iowa & SW 7th         |              |   |
| 2       | O            | 2        | No               | 275     | S Iowa & SW 7th         |              | No Sidewalk, crosswalk running E/W                          |
| 2       | O            | 3        | N/A              |         |                         |              |   |
| 2       | O            | 4        | N/A              |         |                         |              |   |
| 2       | P            | 1        | No               | 272     | S Iowa & S 5th          |              | Sidewalk running to W & S, crosswalk running E/W            |
| 2       | P            | 2        | No               | 271     | S Iowa & S 5th          |              | Sidewalk running N/S, crosswalk running E/W                 |
| 2       | P            | 3        | No               | 273     | S Iowa & S 5th          |              | Sidewalk running N/S  |
| 2       | P            | 4        | N/A              |         |                         |              |   |

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS | Location              | Rating (1-3) | Notes   |
|---------|--------------|----------|------------------|-----|-----------------------|--------------|---|
| 2       | Q            | 1        | No               | 248 | S Iowa & SW 4th       |              | Sidewalk running N & E/W, Crosswalk running N/S   |
| 2       | Q            | 2        | No               | 247 | S Iowa & SW 4th       |              | Sidewalk running N & E/W, Crosswalk running N/S   |
| 2       | Q            | 3        | No               | 246 | S Iowa & SW 4th       |              | Sidewalk running E, & Crosswalk running N/S   |
| 2       | Q            | 4        | No               | 245 | S Iowa & SW 4th       |              | No sidewalk, crosswalk running N/S  |
| 2       | R            | 1        | No               | 286 | S Iowa & SW 3rd       |              | Sidewalk running N/S & E/W, crosswalk running E/W   |
| 2       | R            | 2        | No               | 285 | S Iowa & SW 3rd       |              | Sidewalk running N/S & E/W, crosswalk running E/W   |
| 2       | R            | 3        | No               | 288 | S Iowa & SW 3rd       |              | Sidewalk running N/S & E/W, crosswalk running E/W   |
| 2       | R            | 4        | No               | 287 | S Iowa & SW 3rd       |              | Sidewalk running N/S & E/W, crosswalk running E/W   |
| 2       | S            | 1        | No               | 371 | S Iowa                |              | Sidewalk running N  |
| 2       | S            | 2        | N/A              | 370 | S Iowa                |              | No sidewalk   |
| 2       | S            | 3        | N/A              | 369 | S Iowa                |              | No sidewalk   |
| 2       | S            | 4        | N/A              | 368 | S Iowa                |              | No sidewalk   |
| 2       | T            | 1        | No               | 367 | S Iowa & SW 2nd       |              | Sidewalk running W, no curb/gutter  |
| 2       | T            | 2        | N/A              | 366 | S Iowa & SW 2nd       |              | No sidewalk   |
| 2       | T            | 3        | N/A              | 365 | S Iowa & SW 2nd       |              | No Sidewalk   |
| 2       | T            | 4        | No               | 364 | S Iowa & SW 2nd       |              | Sidewalk running W & S, no curb/gutter  |
| 2       | U            | 1        | No               | 244 | S Nebraska & SW 4th   |              | No sidewalk   |
| 2       | U            | 2        | No               | 243 | S Nebraska & SW 4th   |              | Sidewalk running N & E, crosswalk running N/S   |
| 2       | U            | 3        | No               | 242 | S Nebraska & SW 4th   |              | No sidewalk, crosswalk running N/S  |
| 2       | U            | 4        | No               | 241 | S Nebraska & SW 4th   |              | No sidewalk, crosswalk running N/S  |
| 2       | V            | 1        | No               | 290 | S Nebraska & SW 3rd   |              | Sidewalk running N/S & E/W, crosswalk running N/S & E/W   |
| 2       | V            | 2        | No               | 289 | S Nebraska & SW 3rd   |              | Sidewalk running N/S & E/W, crosswalk running N/S & E/W   |
| 2       | V            | 3        | No               | 292 | S Nebraska & SW 3rd   |              | No sidewalk, crosswalk running N/S & E/W  |
| 2       | V            | 4        | No               | 291 | S Nebraska & SW 3rd   |              | Sidewalk running W, crosswalk running N/S & E/W   |
| 2       | W            | 1        | N/A              | 375 | S Nebraska            |              | No sidewalk   |
| 2       | W            | 2        | No               | 374 | S Nebraska            |              | Sidewalk running N/S  |
| 2       | W            | 3        | No               | 373 | S Nebraska            |              | Sidewalk running N/S  |
| 2       | W            | 4        | No               | 372 | S Nebraska            |              | Sidewalk running S  |
| 2       | X            | 1        | N/A              | 313 | S Nebraska & SW 2nd   |              | No sidewalk   |
| 2       | X            | 2        | No               | 312 | S Nebraska & SW 2nd   |              | Sidewalk running N/S & E  |
| 2       | X            | 3        | No               | 311 | S Nebraska & SW 2nd   |              | Sidewalk running N/S & E  |
| 2       | X            | 4        | N/A              | 310 | S Nebraska & SW 2nd   |              | No sidewalk   |
| 2       | Y            | 1        | No               | 240 | S Kansas Ave & SW 4th |              | No sidewalk, crosswalk running N/S  |
| 2       | Y            | 2        | No               | 239 | S Kansas Ave & SW 4th |              | No sidewalk, crosswalk running N/S  |
| 2       | Y            | 3        | No               | 238 | S Kansas Ave & SW 4th |              | No Sidewalk, crosswalk running N/S  |
| 2       | Y            | 4        | No               | 237 | S Kansas Ave & SW 4th |              | No Sidewalk, crosswalk running N/S  |
| 2       | Z            | 1        | Yes              | 294 | S Kansas & SW 3rd     | 3            | No Truncated Domes, running slope too large, No landing, No traversable path, poor drop-off at flow line                                      |
| 2       | Z            | 2        | Yes              | 293 | S Kansas & SW 3rd     | 3            | No Truncated Domes, Running slope too large, No landing, No traversable path, poor drop-off at flow line                                      |
| 2       | Z            | 3        | No               | 296 | S Kansas & SW 3rd     |              | No sidewalk, crosswalk running N/S & E/W  |
| 2       | Z            | 4        | No               | 295 | S Kansas & SW 3rd     |              | Sidewalk, crosswalk running N/S & E/W   |
| 2       | AA           | 1        | No               | 377 | S Kansas              |              | Vehicle approach  |
| 2       | AA           | 2        | No               | 376 | S Kansas              |              | Vehicle approach  |
| 2       | AA           | 3        | N/A              |     |                       |              |   |
| 2       | AA           | 4        | N/A              |     |                       |              |   |
| 2       | BB           | 1        | No               | 309 | S Kansas Ave & SW 2nd |              | No sidewalk, crosswalk running N/S  |
| 2       | BB           | 2        | No               | 308 | S Kansas Ave & SW 2nd |              | No sidewalk, crosswalk running N/S  |
| 2       | BB           | 3        | No               | 307 | S Kansas Ave & SW 2nd |              | No sidewalk, crosswalk running N/S  |
| 2       | BB           | 4        | Yes              | 306 | S Kansas Ave & SW 2nd | 3            | No Truncated Domes, Throat too narrow, running slope too large, No landing, No traversable path Sidewalk running W & S, crosswalk running N/S |
| 2       | CC           | 1        | N/A              | 381 | S Kansas              |              | No sidewalk   |
| 2       | CC           | 2        | N/A              | 380 | S Kansas              |              | No sidewalk   |
| 2       | CC           | 3        | N/A              | 379 | S Kansas              |              | No sidewalk   |
| 2       | CC           | 4        | N/A              | 378 | S Kansas              |              | No sidewalk   |
| 2       | DD           | 1        | No               | 385 | S Kansas & W 1st      |              | No sidewalk, crosswalk running N/S  |
| 2       | DD           | 2        | No               | 384 | S Kansas & W 1st      |              | No sidewalk, crosswalk running N/S  |
| 2       | DD           | 3        | No               | 383 | S Kansas & W 1st      |              | No sidewalk, crosswalk running N/S  |
| 2       | DD           | 4        | No               | 382 | S Kansas & W 1st      |              | No sidewalk, crosswalk running N/S  |

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS | Location             | Rating (1-3) | Notes  |
|---------|--------------|----------|------------------|-----|----------------------|--------------|--|
| 2       | EE           | 1        | No               | 236 | Colorado & SW 4th    |              | No sidewalk, crosswalk running N/S & E/W   |
| 2       | EE           | 2        | No               | 235 | Colorado & SW 4th    |              | No sidewalk, crosswalk running N/S & E/W   |
| 2       | EE           | 3        | No               | 234 | Colorado & SW 4th    |              | No sidewalk, crosswalk running N/S   |
| 2       | EE           | 4        | No               | 233 | Colorado & SW 4th    |              | No sidewalk, crosswalk running N/S   |
| 2       | FF           | 1        | Yes              | 297 | S Colorado & SW 3rd  | 2            | No truncated domes   |
| 2       | FF           | 2        | No               | 298 | S Colorado & SW 3rd  |              | Sidewalk running E, crosswalk running N/S & E/W  |
| 2       | FF           | 3        | No               | 299 | S Colorado & SW 3rd  |              | No sidewalk, crosswalk running E/W   |
| 2       | FF           | 4        | N/A              |     |                      |              |  |
| 2       | GG           | 1        | No               | 361 | NW 3rd St            |              | Crosswalk running N/S  |
| 2       | GG           | 2        | Yes              | 362 | NW 3rd St            | 3            | Approach is asphalt placed over a pipe, crosswalk running N/S  |
| 2       | GG           | 3        | N/A              |     |                      |              |  |
| 2       | GG           | 4        | N/A              |     |                      |              |  |
| 2       | HH           | 1        | N/A              | 232 | S Utah & SW 4th      |              | No sidewalk  |
| 2       | HH           | 2        | N/A              | 231 | S Utah & SW 4th      |              | No sidewalk  |
| 2       | HH           | 3        | N/A              |     |                      |              |  |
| 2       | HH           | 4        | N/A              |     |                      |              |  |
| 2       | II           | 1        | Yes              | 229 | S Whitley & SW 4th   | 3            | No Truncated Domes, Landing too small, steep landing slope, Narrow traversable path, poor drop-off at flow line, crosswalk running N/S |
| 2       | II           | 2        | Yes              | 230 | S Whitley & SW 4th   | 3            | No Truncated Domes, Landing too small, steep landing & cross slope, Narrow traversable path, crosswalk running N/S                     |
| 2       | II           | 3        | N/A              |     |                      |              |  |
| 2       | II           | 4        | N/A              |     |                      |              |  |
| 2       | JJ           | 1        | Yes              | 300 | S Whitley & SW 3rd   | 3            | No Truncated Domes, Landing too small, Narrow traversable path   |
| 2       | JJ           | 2        | Yes              | 301 | S Whitley & SW 3rd   | 3            | No Truncated Domes, Landing too small, No concrete traversable path, crosswalk running N/S   |
| 2       | JJ           | 3        | Yes              | 302 | S Whitley & SW 3rd   | 3            | No Truncated Domes, Landing too small, No concrete traversable path  |
| 2       | JJ           | 4        | Yes              | 303 | S Whitley & SW 3rd   | 3            | No Truncated Domes, steep running slope, Landing too small, No concrete traversable path, poor drop-off at flow line                   |
| 2       | KK           | 1        | Yes              |     | S Whitley & SW 2nd   | 3            | No Truncated Domes, Landing too small, steep running & cross slope, No concrete traversable path, poor drop-off at flow line           |
| 2       | KK           | 2        | Yes              |     | S Whitley & SW 2nd   | 3            | No Truncated Domes, Narrow throat, steep running slope, Landing too small, Need wider traversable path, poor drop-off at flow line     |
| 2       | KK           | 3        | N/A              |     |                      |              |  |
| 2       | KK           | 4        | N/A              |     |                      |              |  |
| 2       | LL           | 1        | Yes              | 593 | E 1st & Paradise Crt | 3            | No truncated domes, narrow throat, small landing, narrow traversable path  |
| 2       | LL           | 2        | Yes              | 594 | E 1st & Paradise Crt | 3            | No truncated domes, narrow throat, small landing, narrow traversable path  |
| 2       | LL           | 3        | N/A              |     |                      |              |  |
| 2       | LL           | 4        | N/A              |     |                      |              |  |

|   |   |   |     |     |                          |   |  |
|---|---|---|-----|-----|--------------------------|---|--|
| 3 | A | 1 | Yes | 405 | NW 7th                   | 1 |  |
| 3 | A | 2 | Yes | 406 | NW 7th                   | 3 | Steep running & landing slope  |
| 3 | A | 3 | N/A |     |                          |   |  |
| 3 | A | 4 | N/A |     |                          |   |  |
| 3 | B | 1 | Yes | 351 | N Whitley & NW 7th       | 1 | Crosswalk running N/S  |
| 3 | B | 2 | N/A |     |                          |   | Asphalt pathway  |
| 3 | B | 3 | N/A |     |                          |   | Asphalt pathway  |
| 3 | B | 4 | Yes | 350 | N Whitley & NW 7th       | 1 | Crosswalk running N/S  |
| 3 | C | 1 | N/A | 616 | N Whitley & NW 4th       |   | Commercial Approach  |
| 3 | C | 2 | Yes | 314 | N Whitley & NW 4th       | 3 | No Truncated Domes, steep running slope, No landing, No traversable path, rough drop-off at flow line, Crosswalk running N/S |
| 3 | C | 3 | Yes | 315 | N Whitley & NW 4th       | 3 | No Truncated Domes, steep running slope, No landing, No traversable path, rough drop-off at flow line, Crosswalk running N/S |
| 3 | C | 4 | N/A | 617 | N Whitley & NW 4th       |   | Commercial Approach  |
| 3 | D | 1 | No  | 396 | N Whitley & NW 3rd       |   | Sidewalk running N/S, crosswalk running N/S  |
| 3 | D | 2 | Yes | 618 | N Whitley & NW 3rd       | 3 | Crosswalk running N/S, asphalt path  |
| 3 | D | 3 | Yes | 619 | N Whitley & NW 3rd       | 3 | Crosswalk running N/S, asphalt path  |
| 3 | D | 4 | Yes | 395 | N Whitley & NW 3rd       | 2 | Sidewalk running N/S & W, crosswalk running N/S  |
| 3 | E | 1 | Yes | 393 | N Whitley & Orchard Park | 3 | No Truncated Domes, narrow throat, No landing, No traversable path   |
| 3 | E | 2 | Yes | 394 | N Whitley & Orchard Park | 3 | No Truncated Domes, steep cross slope, No landing, No traversable path   |
| 3 | E | 3 | N/A |     |                          |   |  |
| 3 | E | 4 | N/A |     |                          |   |  |

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS | Location                  | Rating (1-3) | Notes   |
|---------|--------------|----------|------------------|-----|---------------------------|--------------|---|
| 3       | F            | 1        | Yes              | 349 | N Whitley & NW 2nd        | 3            | No Truncated Domes, steep running slope, No landing, No traversable path, rough drop-off at flow line       |
| 3       | F            | 2        | Yes              | 348 | N Whitley & NW 2nd        | 2            | No Truncated Domes, No landing, No traversable path   |
| 3       | F            | 3        | N/A              |     |                           |              |   |
| 3       | F            | 4        | N/A              |     |                           |              |   |
| 3       | G            | 1        | Yes              | 344 | S Whitley & W 1st         | 3            | No Truncated Domes, steep cross slope, small landing, narrow traversable path, crosswalk running N/S        |
| 3       | G            | 2        | Yes              | 346 | S Whitley & W 1st         | 3            | No Truncated Domes, steep running slope, small landing, narrow traversable path, crosswalk running N/S      |
| 3       | G            | 3        | Yes              | 347 | S Whitley & W 1st         | 3            | No Truncated Domes, steep running slope, small landing, narrow traversable path, crosswalk running N/S      |
| 3       | G            | 4        | No               | 345 | S Whitley & W 1st         |              | crosswalk running N/S   |
| 3       | H            | 1        | Yes              | 404 | Quail Cove & Bobwhite     | 3            | No truncated domes, narrow throat, steep landing and running slopes, landing too small, no traversable path |
| 3       | H            | 2        | Yes              | 403 | Quail Cove & Bobwhite     | 2            | No truncated domes, landing too small, no traversable path  |
| 3       | H            | 3        | N/A              |     |                           |              |   |
| 3       | H            | 4        | N/A              |     |                           |              |   |
| 3       | I            | 1        | Yes              | 402 | Quail Cove & NW 3rd       | 2            | No truncated domes, landing too small, narrow traversable path  |
| 3       | I            | 2        | Yes              | 401 | Quail Cove & NW 3rd       | 2            | No truncated domes, landing too small, narrow traversable path  |
| 3       | I            | 3        | Yes              | 400 | Quail Cove & NW 3rd       | 2            | No truncated domes, landing too small, narrow traversable path  |
| 3       | I            | 4        | Yes              | 399 | Quail Cove & NW 3rd       | 2            | No truncated domes, landing too small, narrow traversable path  |
| 3       | J            | 1        | N/A              | 399 | Washington & NW 3rd       |              | sidewalk ends   |
| 3       | J            | 2        | N/A              | 398 | Washington & NW 3rd       |              | no sidewalk   |
| 3       | J            | 3        | Yes              | 397 | Washington & NW 3rd       | 2            | No truncated domes, narrow landing, no traversable path   |
| 3       | J            | 4        | N/A              |     |                           |              |   |
| 3       | K            | 1        | No               | 390 | Washington & Orchard Park |              | No sidewalk   |
| 3       | K            | 2        | N/A              | 389 | Washington & Orchard Park |              | No sidewalk   |
| 3       | K            | 3        | N/A              |     |                           |              |   |
| 3       | K            | 4        | N/A              |     |                           |              |   |
| 3       | L            | 1        | N/A              | 387 | Washington & NW 2nd       |              | Sidewalk in poor shape  |
| 3       | L            | 2        | N/A              | 386 | Washington & NW 2nd       |              | Sidewalk in poor shape  |
| 3       | L            | 3        | N/A              |     |                           |              |   |
| 3       | L            | 4        | N/A              |     |                           |              |   |
| 3       | M            | 1        | Yes              | 343 | Wilde Rose Circle & W 1st | 3            | No Truncated Domes, steep landing & cross slopes, small landing, no traversable path                        |
| 3       | M            | 2        | Yes              | 342 | Wilde Rose Circle & W 1st | 3            | No Truncated Domes, steep landing & cross slopes, small landing, no traversable path                        |
| 3       | M            | 3        | N/A              |     |                           |              |   |
| 3       | M            | 4        | N/A              |     |                           |              |   |
| 3       | N            | 1        | N/A              | 316 | N Utah & NW 4th           |              | No sidewalks  |
| 3       | N            | 2        | No               | 317 | N Utah & NW 4th           |              | sidewalk  |
| 3       | N            | 3        | N/A              |     |                           |              |   |
| 3       | N            | 4        | N/A              |     |                           |              |   |
| 3       | O            | 1        | N/A              | 318 | N Dakota Ave & NW 4th     |              | No sidewalks  |
| 3       | O            | 2        | N/A              | 319 | N Dakota Ave & NW 4th     |              | No sidewalks  |
| 3       | O            | 3        | N/A              |     |                           |              |   |
| 3       | O            | 4        | N/A              |     |                           |              |   |
| 3       | P            | 1        | N/A              | 320 | N Kansas & NW 4th         |              | No sidewalks  |
| 3       | P            | 2        | N/A              | 321 | N Kansas & NW 4th         |              | No sidewalks  |
| 3       | P            | 3        | N/A              |     |                           |              |   |
| 3       | P            | 4        | N/A              |     |                           |              |   |
| 3       | Q            | 1        | Yes              | 322 | W Crimson Circle & NW 4th | 3            | No Truncated Domes, Steep running slope, No landing, No traversable path                                    |
| 3       | Q            | 2        | N/A              |     |                           |              |   |
| 3       | Q            | 3        | N/A              |     |                           |              |   |
| 3       | Q            | 4        | N/A              |     |                           |              |   |
| 3       | R            | 1        | Yes              | 329 | W Crimson Circle & NW 4th | 2            | No Truncated Domes, No landing, No traversable path   |
| 3       | R            | 2        | Yes              | 330 | W Crimson Circle & NW 4th | 2            | No Truncated Domes, No landing, No traversable path   |
| 3       | R            | 3        | N/A              |     |                           |              |   |
| 3       | R            | 4        | N/A              |     |                           |              |   |
| 3       | S            | 1        | Yes              | 326 | E Crimson Circle          | 3            | Narrow throat, Narrow landing, no traversable path  |
| 3       | S            | 2        | Yes              | 325 | E Crimson Circle          | 3            | Narrow throat, Narrow landing, no traversable path  |
| 3       | S            | 3        | N/A              |     |                           |              |   |
| 3       | S            | 4        | N/A              |     |                           |              |   |

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS | Location                        | Rating (1-3) | Notes   |
|---------|--------------|----------|------------------|-----|---------------------------------|--------------|---|
| 3       | T            | 1        | N/A              | 336 | N Kansas & NW 3rd               |              | No sidewalks  |
| 3       | T            | 2        | Yes              | 335 | N Kansas & NW 3rd               | 3            | No Truncated Domes, Narrow throat, No landing, No traversable path, rough drop-off at flow line               |
| 3       | T            | 3        | Yes              | 334 | N Kansas & NW 3rd               | 2            | No Truncated Domes, No landing, No traversable path, rough drop-off at flow line                              |
| 3       | T            | 4        | N/A              | 333 | N Kansas & NW 3rd               |              | No sidewalks  |
| 3       | U            | 1        | Yes              | 331 | W Crimson Circle & NW 3rd       | 3            | No Truncated Domes, Narrow throat, steep running slope, No landing, No traversable path                       |
| 3       | U            | 2        | Yes              | 332 | W Crimson Circle & NW 3rd       | 3            | No Truncated Domes, steep running slope, No landing, No traversable path                                      |
| 3       | U            | 3        | N/A              |     |                                 |              |   |
| 3       | U            | 4        | N/A              |     |                                 |              |   |
| 3       | V            | 1        | Yes              | 327 | E Crimson Circle                | 3            | Narrow throat, Narrow landing, no traversable path  |
| 3       | V            | 2        | Yes              | 328 | E Crimson Circle                | 3            | Narrow throat, Narrow landing, no traversable path  |
| 3       | V            | 3        | N/A              |     |                                 |              |   |
| 3       | V            | 4        | N/A              |     |                                 |              |   |
| 3       | W            | 1        | N/A              |     |                                 |              |   |
| 3       | W            | 2        | N/A              |     |                                 |              |   |
| 3       | W            | 3        | N/A              |     |                                 |              |   |
| 3       | W            | 4        | N/A              |     |                                 |              |   |
| 3       | X            | 1        | Yes              | 337 | N Utah & NW 2nd                 | 3            | No Truncated Domes, steep running & cross slope, No landing, No traversable path, rough drop-off at flow line |
| 3       | X            | 2        | Yes              | 338 | N Utah & NW 2nd                 | 3            | No Truncated Domes, No landing, No traversable path   |
| 3       | X            | 3        | N/A              |     |                                 |              |   |
| 3       | X            | 4        | N/A              |     |                                 |              |   |
| 3       | Y            | 1        | N/A              | 340 | N Utah & W 1st                  |              | No sidewalks  |
| 3       | Y            | 2        | N/A              | 341 | N Utah & W 1st                  |              | No sidewalks  |
| 3       | Y            | 3        | N/A              |     |                                 |              |   |
| 3       | Y            | 4        | N/A              |     |                                 |              |   |
| 3       | Z            | 1        | No               | 429 | N Pennsylvania & NW 9th         |              | No sidewalk at corner   |
| 3       | Z            | 2        | No               | 428 | N Pennsylvania & NW 9th         |              | No sidewalk at corner   |
| 3       | Z            | 3        | N/A              |     |                                 |              |   |
| 3       | Z            | 4        | N/A              |     |                                 |              |   |
| 3       | AA           | 1        | N/A              | 427 | N Pennsylvania & NE 8th         |              | No sidewalk   |
| 3       | AA           | 2        | Yes              | 426 | N Pennsylvania & NE 8th         | 1            |   |
| 3       | AA           | 3        | N/A              |     |                                 |              |   |
| 3       | AA           | 4        | N/A              |     |                                 |              |   |
| 3       | BB           | 1        | Yes              | 413 | N Pennsylvania & Magnolia       | 3            | Steep landing slope, crosswalk running N/S  |
| 3       | BB           | 2        | Yes              | 412 | N Pennsylvania & Magnolia       | 1            | crosswalk running N/S   |
| 3       | BB           | 3        | N/A              |     |                                 |              |   |
| 3       | BB           | 4        | N/A              |     |                                 |              |   |
| 3       | CC           | 1        | Yes              | 411 | N Pennsylvania & Syringia Falls | 3            | Steep running slope, crosswalk running N/S  |
| 3       | CC           | 2        | Yes              | 410 | N Pennsylvania & Syringia Falls | 3            | Steep cross slope, crosswalk running N/S  |
| 3       | CC           | 3        | N/A              |     |                                 |              |   |
| 3       | CC           | 4        | N/A              |     |                                 |              |   |
| 3       | DD           | 1        | N/A              | 409 | N Pennsylvania & NW 4th         |              | No sidewalk   |
| 3       | DD           | 2        | N/A              | 408 | N Pennsylvania & NW 4th         |              | No sidewalk   |
| 3       | DD           | 3        | N/A              |     |                                 |              |   |
| 3       | DD           | 4        | N/A              |     |                                 |              |   |
| 3       | EE           | 1        | No               |     | N Pennsylvania                  |              | Commercial Approach   |
| 3       | EE           | 2        | No               |     |                                 |              | Commercial Approach   |
| 3       | EE           | 3        | N/A              |     |                                 |              |   |
| 3       | EE           | 4        | N/A              |     |                                 |              |   |
| 3       | FF           | 1        | Yes              | 407 | N Pennsylvania                  | 3            | No Truncated Domes, narrow throat, steep running slope, narrow landing & traversable path                     |
| 3       | FF           | 2        | N/A              |     |                                 |              |   |
| 3       | FF           | 3        | N/A              |     |                                 |              |   |
| 3       | FF           | 4        | N/A              |     |                                 |              |   |
| 3       | GG           | 1        | Yes              | 414 | Syringia Springs & Magnolia     | 1            |   |
| 3       | GG           | 2        | Yes              | 415 | Syringia Springs & Magnolia     | 1            |   |
| 3       | GG           | 3        | N/A              |     |                                 |              |   |
| 3       | GG           | 4        | N/A              |     |                                 |              |   |

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS | Location                        | Rating (1-3) | Notes |
|---------|--------------|----------|------------------|-----|---------------------------------|--------------|-------|
| 3       | HH           | 1        | Yes              | 422 | Lilac & Magnolia                | 1            |       |
| 3       | HH           | 2        | Yes              | 423 | Lilac & Magnolia                | 1            |       |
| 3       | HH           | 3        | N/A              |     |                                 |              |       |
| 3       | HH           | 4        | N/A              |     |                                 |              |       |
| 3       | II           | 1        | Yes              | 424 | Zinnia & Magnolia               | 1            |       |
| 3       | II           | 2        | Yes              | 425 | Zinnia & Magnolia               | 1            |       |
| 3       | II           | 3        | N/A              |     |                                 |              |       |
| 3       | II           | 4        | N/A              |     |                                 |              |       |
| 3       | JJ           | 1        | Yes              | 416 | Syringia Springs & Honey Suckle | 1            |       |
| 3       | JJ           | 2        | Yes              | 417 | Syringia Springs & Honey Suckle | 1            |       |
| 3       | JJ           | 3        | N/A              |     |                                 |              |       |
| 3       | JJ           | 4        | N/A              |     |                                 |              |       |
| 3       | KK           | 1        | Yes              | 420 | Lilac & Honey Suckle            | 1            |       |
| 3       | KK           | 2        | Yes              | 421 | Lilac & Honey Suckle            | 1            |       |
| 3       | KK           | 3        | N/A              |     |                                 |              |       |
| 3       | KK           | 4        | N/A              |     |                                 |              |       |
| 3       | LL           | 1        | Yes              | 418 | Syringia Springs & Orchid       | 1            |       |
| 3       | LL           | 2        | Yes              | 419 | Syringia Springs & Orchid       | 1            |       |
| 3       | LL           | 3        | N/A              |     |                                 |              |       |
| 3       | LL           | 4        | N/A              |     |                                 |              |       |

|   |   |   |     |     |                     |   |  |
|---|---|---|-----|-----|---------------------|---|--|
| 4 | A | 1 | Yes | 523 | N Whitley & NW 16th | 3 | No truncated domes, narrow throat, narrow landing & traversable path       |
| 4 | A | 2 | Yes | 520 | N Whitley & NW 16th | 3 | No truncated domes, steep running slope, narrow landing & traversable path |
| 4 | A | 3 | Yes | 521 | N Whitley & NW 16th | 3 | No truncated domes, steep running slope, narrow landing & traversable path |
| 4 | A | 4 | Yes | 522 | N Whitley & NW 16th | 2 | No truncated domes   |
| 4 | B | 1 | No  | 478 | N Whitley & NW 13th |   | sidewalk but no approach   |
| 4 | B | 2 | Yes | 479 | N Whitley & NW 13th | 3 | No truncated domes, steep running slope, poor drop-off at flow line        |
| 4 | B | 3 | N/A |     |                     |   |  |
| 4 | B | 4 | N/A |     |                     |   |  |
| 4 | C | 1 | Yes | 480 | N Whitley & NW 12th | 2 | No truncated domes, no landing or traversable path                         |
| 4 | C | 2 | Yes | 481 | N Whitley & NW 12th | 2 | No truncated domes, no landing or traversable path                         |
| 4 | C | 3 | N/A |     |                     |   |  |
| 4 | C | 4 | N/A |     |                     |   |  |
| 4 | D | 1 | Yes | 487 | N Whitley & NW 11th | 2 | No truncated domes, narrow landing & traversable path                      |
| 4 | D | 2 | N/A |     |                     |   |  |
| 4 | D | 3 | N/A |     |                     |   |  |
| 4 | D | 4 | N/A |     |                     |   |  |
| 4 | E | 1 | Yes | 484 | N Vermont & NW 12th | 2 | steep drop-off at flow line  |
| 4 | E | 2 | N/A | 483 | N Vermont & NW 12th |   | No sidewalk  |
| 4 | E | 3 | N/A |     |                     |   |  |
| 4 | E | 4 | N/A |     |                     |   |  |
| 4 | F | 1 | Yes | 485 | N Vermont & NW 11th | 2 | No truncated domes, no landing or traversable path                         |
| 4 | F | 2 | Yes | 486 | N Vermont & NW 11th | 2 | No truncated domes, no landing or traversable path                         |
| 4 | F | 3 | N/A |     |                     |   |  |
| 4 | F | 4 | N/A |     |                     |   |  |
| 4 | G | 1 | Yes | 613 | NW 16th             | 1 | Ramp & landing narrow  |
| 4 | G | 2 | Yes | 611 | NW 16th             | 1 | Ramp & landing narrow, large cross slope                                   |
| 4 | G | 3 | N/A |     |                     |   |  |
| 4 | G | 4 | N/A |     |                     |   |  |
| 4 | H | 1 | Yes | 610 | NW 16th             | 1 | Ramp & landing narrow, large cross slope & landing slope                   |
| 4 | H | 2 | Yes | 609 | NW 16th             | 1 | Ramp & landing narrow, large cross slope & landing slope                   |
| 4 | H | 3 | N/A |     |                     |   |  |
| 4 | H | 4 | N/A |     |                     |   |  |
| 4 | I | 1 | Yes | 602 | NW 16th & Ash Loop  | 1 | No truncated domes, steep cross slope                                      |
| 4 | I | 2 | Yes | 601 | NW 16th & Ash Loop  | 2 | No truncated domes   |
| 4 | I | 3 | N/A |     |                     |   |  |
| 4 | I | 4 | N/A |     |                     |   |  |

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS | Location                 | Rating (1-3) | Notes   |
|---------|--------------|----------|------------------|-----|--------------------------|--------------|---|
| 4       | J            | 1        | N/A              | 603 | Ash Loop                 |              |   |
| 4       | J            | 2        | N/A              | 604 | Ash Loop                 |              |   |
| 4       | J            | 3        | N/A              |     |                          |              |   |
| 4       | J            | 4        | N/A              |     |                          |              |   |
| 4       | K            | 1        | N/A              | 605 | Ash Loop                 |              |   |
| 4       | K            | 2        | N/A              | 606 | Ash Loop                 |              |   |
| 4       | K            | 3        | N/A              |     |                          |              |   |
| 4       | K            | 4        | N/A              |     |                          |              |   |
| 4       | L            | 1        | N/A              | 607 | NW 16th                  |              |   |
| 4       | L            | 2        | N/A              | 608 | NW 16th                  |              |   |
| 4       | L            | 3        | N/A              |     |                          |              |   |
| 4       | L            | 4        | N/A              |     |                          |              |   |
| 4       | M            | 1        | Yes              | 600 | NW 16th & Ash Loop       | 2            | No truncated domes  |
| 4       | M            | 2        | Yes              | 599 | NW 16th & Ash Loop       | 3            | No truncated domes, steep running & landing slope   |
| 4       | M            | 3        | N/A              |     |                          |              |   |
| 4       | M            | 4        | N/A              |     |                          |              |   |
| 4       | N            | 1        | Yes              | 471 | N Pennsylvania & NE 16th | 2            | No truncated domes, narrow landing & traversable path                                     |
| 4       | N            | 2        | N/A              | 470 | N Pennsylvania & NE 16th |              | No sidewalk   |
| 4       | N            | 3        | N/A              | 469 | N Pennsylvania & NE 16th |              | No sidewalk   |
| 4       | N            | 4        | N/A              | 468 | N Pennsylvania & NE 16th |              | No sidewalk   |
| 4       | O            | 1        | Yes              | 473 | N Pennsylvania & Oak     | 3            | Narrow throat, narrow landing & traversable path, steep landing slope                     |
| 4       | O            | 2        | Yes              | 472 | N Pennsylvania & Oak     | 3            | Narrow throat, narrow landing & traversable path  |
| 4       | O            | 3        | N/A              |     |                          |              |   |
| 4       | O            | 4        | N/A              |     |                          |              |   |
| 4       | P            | 1        | Yes              | 475 | Oak Ave                  | 3            | No truncated domes, steep landing slope   |
| 4       | P            | 2        | Yes              | 474 | Oak Ave                  | 2            | No truncated domes  |
| 4       | P            | 3        | N/A              |     |                          |              |   |
| 4       | P            | 4        | N/A              |     |                          |              |   |
| 4       | Q            | 1        | Yes              | 477 | Walnut & Beech           | 2            | No truncated domes  |
| 4       | Q            | 2        | Yes              | 476 | Walnut & Beech           | 2            | No truncated domes  |
| 4       | Q            | 3        | N/A              |     |                          |              |   |
| 4       | Q            | 4        | N/A              |     |                          |              |   |
| 4       | R            | 1        | No               | 467 | Burton & NE 16th         |              | sidewalk but no approach  |
| 4       | R            | 2        | No               | 466 | Burton & NE 16th         |              |   |
| 4       | R            | 3        | N/A              |     |                          |              |   |
| 4       | R            | 4        | N/A              |     |                          |              |   |
| 4       | S            | 1        | No               | 463 | Lucile & NE 16th         |              | sidewalk but no approach  |
| 4       | S            | 2        | No               | 462 | Lucile & NE 16th         |              | sidewalk but no approach  |
| 4       | S            | 3        | N/A              |     |                          |              |   |
| 4       | S            | 4        | N/A              |     |                          |              |   |
| 4       | T            | 1        | Yes              | 461 | Jessica & NE 16th        | 3            | No truncated domes, narrow throat, narrow landing & traversable path                      |
| 4       | T            | 2        | Yes              | 598 | Jessica & NE 16th        | 3            | No truncated domes, narrow throat, narrow landing & traversable path                      |
| 4       | T            | 3        | N/A              |     |                          |              |   |
| 4       | T            | 4        | N/A              |     |                          |              |   |
| 4       | U            | 1        | Yes              | 459 | Jessica & NE 13th        | 3            | No truncated domes, narrow throat, narrow landing & traversable path                      |
| 4       | U            | 2        | Yes              | 460 | Jessica & NE 13th        | 3            | No truncated domes, narrow throat, steep running slope, narrow landing & traversable path |
| 4       | U            | 3        | N/A              |     |                          |              |   |
| 4       | U            | 4        | N/A              |     |                          |              |   |

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS | Location                 | Rating (1-3) | Notes  |
|---------|--------------|----------|------------------|-----|--------------------------|--------------|--|
| 4       | V            | 1        | Yes              | 458 | Tara Ct & NE 13th        | 3            | No truncated domes, steep landing slope, no landing or traversable path          |
| 4       | V            | 2        | Yes              | 457 | Tara Ct & NE 13th        | 3            | No truncated domes, steep landing slope, no landing or traversable path          |
| 4       | V            | 3        | Yes              | 456 | Tara Ct & NE 13th        | 2            | No truncated domes, no landing or traversable path                               |
| 4       | V            | 4        | Yes              | 455 | Tara Ct & NE 13th        | 2            | No truncated domes, no landing or traversable path                               |
| 4       | W            | 1        | Yes              | 453 | N Chaney & NE 13th       | 3            | No truncated domes, narrow throat, no landing or traversable path                |
| 4       | W            | 2        | Yes              | 454 | N Chaney & NE 13th       | 3            | No truncated domes, steep running slope, no landing, no traversable path         |
| 4       | W            | 3        | N/A              |     |                          |              |  |
| 4       | W            | 4        | N/A              |     |                          |              |  |
| 4       | X            | 1        | N/A              | 449 | N Chaney & NE 14th       |              |  |
| 4       | X            | 2        | N/A              | 450 | N Chaney & NE 14th       |              |  |
| 4       | X            | 3        | N/A              | 451 | N Chaney & NE 14th       |              |  |
| 4       | X            | 4        | N/A              |     |                          |              |  |
| 4       | Y            | 1        | No               | 447 | Lucile & NE 14th         |              |  |
| 4       | Y            | 2        | Yes              | 448 | Lucile & NE 14th         | 2            | No truncated domes, no landing or traversable path                               |
| 4       | Y            | 3        | N/A              |     |                          |              |  |
| 4       | Y            | 4        | N/A              |     |                          |              |  |
| 4       | Z            | 1        | N/A              | 464 | Lucile & NE 15th         |              |  |
| 4       | Z            | 2        | N/A              | 465 | Lucile & NE 15th         |              |  |
| 4       | Z            | 3        | N/A              |     |                          |              |  |
| 4       | Z            | 4        | N/A              |     |                          |              |  |
| 4       | AA           | 1        | No               | 445 | Burton & NE 14th         |              |  |
| 4       | AA           | 2        | No               | 446 | Burton & NE 14th         |              |  |
| 4       | AA           | 3        | N/A              |     |                          |              |  |
| 4       | AA           | 4        | N/A              |     |                          |              |  |
| 4       | BB           | 1        | No               | 444 | N Pennsylvania & NE 14th |              |  |
| 4       | BB           | 2        | Yes              | 443 | N Pennsylvania & NE 14th | 3            | No truncated domes, steep running slope, narrow landing & traversable path       |
| 4       | BB           | 3        | N/A              |     |                          |              |  |
| 4       | BB           | 4        | N/A              |     |                          |              |  |
| 4       | CC           | 1        | N/A              | 437 | N Pennsylvania & NE 12th |              | No sidewalk  |
| 4       | CC           | 2        | Yes              | 436 | N Pennsylvania & NE 12th | 3            | No truncated domes, steep running slope, no landing, no traversable path         |
| 4       | CC           | 3        | Yes              | 435 | N Pennsylvania & NE 12th | 3            | No truncated domes, steep running slope, no landing, no traversable path         |
| 4       | CC           | 4        | Yes              | 434 | N Pennsylvania & NE 12th | 3            | No truncated domes, no landing, no traversable path                              |
| 4       | CC           | 5        | Yes              | 433 | N Pennsylvania & NE 12th | 3            | No truncated domes, steep running & cross slope, no landing, no traversable path |
| 4       | CC           | 6        | N/A              | 438 | N Pennsylvania & NE 12th |              | No sidewalk  |
| 4       | DD           | 1        | N/A              | 432 | N Pennsylvania & 11th    |              |  |
| 4       | DD           | 2        | N/A              | 431 | N Pennsylvania & 11th    |              |  |
| 4       | DD           | 3        | N/A              |     |                          |              |  |
| 4       | DD           | 4        | N/A              |     |                          |              |  |

|   |   |   |     |     |                     |   |   |
|---|---|---|-----|-----|---------------------|---|---|
| 5 | A | 1 | Yes | 524 | N Arizona & NW 16th | 2 | No truncated domes, no landing or traversable path                      |
| 5 | A | 2 | Yes | 525 | N Arizona & NW 16th | 3 | No truncated domes, steep running slope, no landing or traversable path |
| 5 | A | 3 | N/A |     |                     |   |   |
| 5 | A | 4 | N/A |     |                     |   |   |

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS     | Location           | Rating (1-3) | Notes   |
|---------|--------------|----------|------------------|---------|--------------------|--------------|---|
| 5       | B            | 1        | Yes              | 567/568 | Allen & NW 16th    | 1            |   |
| 5       | B            | 3        | Yes              | 562/561 | Allen & NW 16th    | 1            |   |
| 5       | B            | 5        | Yes              | 563/564 | Allen & NW 16th    | 1            |   |
| 5       | B            | 7        | Yes              | 565/566 | Allen & NW 16th    | 1            |   |
| 5       | C            | 1        | Yes              | 570     | Allen & NW 17th    | 3            | steep cross & landing slope                                       |
| 5       | C            | 2        | Yes              | 569     | Allen & NW 17th    | 1            |   |
| 5       | C            | 3        | N/A              |         |                    |              |   |
| 5       | C            | 4        | N/A              |         |                    |              |   |
| 5       | D            | 1        | N/A              | 627     | Allen & NW 18th    |              |   |
| 5       | D            | 2        | N/A              | 628     | Allen & NW 18th    |              |   |
| 5       | D            | 3        | N/A              |         |                    |              |   |
| 5       | D            | 4        | N/A              |         |                    |              |   |
| 5       | E            | 1        | Yes              | 572     | Allen & NW 19th    | 1            |   |
| 5       | E            | 2        | Yes              | 571     | Allen & NW 19th    | 1            |   |
| 5       | E            | 3        | N/A              |         |                    |              |   |
| 5       | E            | 4        | N/A              |         |                    |              |   |
| 5       | F            | 1        | Yes              | 574     | Allen & NW 20th    | 3            | steep running & cross slope                                       |
| 5       | F            | 2        | Yes              | 575     | Allen & NW 20th    | 1            |   |
| 5       | F            | 3        | Yes              | 573     | Allen & NW 20th    | 1            |   |
| 5       | F            | 4        | N/A              |         |                    |              |   |
| 5       | G            | 1        | Yes              | 625     | Allen              | 1            |   |
| 5       | G            | 2        | Yes              | 626     | Allen              | 3            | steep running slope   |
| 5       | G            | 3        | N/A              |         |                    |              |   |
| 5       | G            | 4        | N/A              |         |                    |              |   |
| 5       | H            | 1        | Yes              | 560     | Washoe & Allen     | 1            |   |
| 5       | H            | 2        | Yes              | 559     | Washoe & Allen     | 1            |   |
| 5       | H            | 3        | N/A              |         |                    |              |   |
| 5       | H            | 4        | N/A              |         |                    |              |   |
| 5       | I            | 1        | Yes              | 557     | Washoe & Poplar    | 1            |   |
| 5       | I            | 2        | Yes              | 558     | Washoe & Poplar    | 3            | Steep running & landing slopes, narrow landing & traversable path |
| 5       | I            | 3        | N/A              |         |                    |              |   |
| 5       | I            | 4        | N/A              |         |                    |              |   |
| 5       | J            | 2        | Yes              | 556     | Dogwood & Poplar   | 1            |   |
| 5       | J            | 3        | Yes              | 553     | Dogwood & Poplar   | 1            |   |
| 5       | J            | 4        | Yes              | 554     | Dogwood & Poplar   | 1            |   |
| 5       | J            | 1        | Yes              | 555     | Dogwood & Poplar   | 1            |   |
| 5       | K            | 1        | Yes              | 552     | Bayberry & Poplar  | 1            |   |
| 5       | K            | 2        | Yes              | 553     | Bayberry & Poplar  | 3            | Steep landing slope   |
| 5       | K            | 3        | N/A              |         |                    |              |   |
| 5       | K            | 4        | N/A              |         |                    |              |   |
| 5       | L            | 1        | Yes              | 549     | Spruce & Poplar    | 3            | steep cross & landing slopes                                      |
| 5       | L            | 2        | Yes              | 550     | Spruce & Poplar    | 1            |   |
| 5       | L            | 3        | N/A              |         |                    |              |   |
| 5       | L            | 4        | N/A              |         |                    |              |   |
| 5       | M            | 1        | Yes              | 548     | Spruce & Birch     | 1            |   |
| 5       | M            | 2        | Yes              | 547     | Spruce & Birch     | 1            |   |
| 5       | M            | 3        | N/A              |         |                    |              |   |
| 5       | M            | 4        | N/A              |         |                    |              |   |
| 5       | N            | 1        | Yes              | 545     | N Whitley & Spruce | 1            |   |
| 5       | N            | 2        | Yes              | 546     | N Whitley & Spruce | 3            | steep landing slope   |
| 5       | N            | 3        | N/A              |         |                    |              |   |
| 5       | N            | 4        | N/A              |         |                    |              |   |

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS | Location            | Rating (1-3) | Notes   |
|---------|--------------|----------|------------------|-----|---------------------|--------------|---|
| 5       | O            | 1        | N/A              |     |                     |              |   |
| 5       | O            | 2        | N/A              |     |                     |              |   |
| 5       | O            | 3        | N/A              |     |                     |              |   |
| 5       | O            | 4        | N/A              |     |                     |              |   |
| 5       | P            | 1        | N/A              |     |                     |              |   |
| 5       | P            | 2        | N/A              |     |                     |              |   |
| 5       | P            | 3        | N/A              |     |                     |              |   |
| 5       | P            | 4        | N/A              |     |                     |              |   |
| 5       | Q            | 1        | N/A              |     |                     |              |   |
| 5       | Q            | 2        | N/A              |     |                     |              |   |
| 5       | Q            | 3        | N/A              |     |                     |              |   |
| 5       | Q            | 4        | N/A              |     |                     |              |   |
| 5       | R            | 1        | Yes              | 528 | N Whitley & NW 24th | 3            | No truncated domes, steep running slope, no landing or traversable path                                     |
| 5       | R            | 2        | N/A              | 544 | N Whitley & NW 24th |              | No sidewalk   |
| 5       | R            | 3        | Yes              | 543 | N Whitley & NW 24th | 2            | No truncated domes, No landing or traversable path, poor drop-off at flow line                              |
| 5       | R            | 4        | Yes              | 526 | N Whitley & NW 24th | 2            | No truncated domes, No landing or traversable path  |
| 5       | S            | 1        | Yes              | 541 | Golden & NW 24th    | 2            | No truncated domes, no landing or traversable path  |
| 5       | S            | 2        | Yes              | 542 | Golden & NW 24th    | 2            | No truncated domes, no landing or traversable path  |
| 5       | S            | 3        | N/A              |     |                     |              |   |
| 5       | S            | 4        | N/A              |     |                     |              |   |
| 5       | T            | 1        | Yes              | 539 | Golden & Jonathan   | 2            | No truncated domes, no landing or traversable path  |
| 5       | T            | 2        | Yes              | 540 | Golden & Jonathan   | 2            | No truncated domes, no landing or traversable path  |
| 5       | T            | 3        | N/A              |     |                     |              |   |
| 5       | T            | 4        | N/A              |     |                     |              |   |
| 5       | U            | 1        | Yes              | 537 | Golden & Braeburn   | 3            | No truncated domes, narrow throat, steep running slope, no landing or traversable path                      |
| 5       | U            | 2        | Yes              | 538 | Golden & Braeburn   | 3            | No truncated domes, narrow throat, steep running slope, no landing or traversable path                      |
| 5       | U            | 3        | N/A              |     |                     |              |   |
| 5       | U            | 4        | N/A              |     |                     |              |   |
| 5       | V            | 1        | Yes              | 535 | Winesap & Golden    | 3            | No truncated domes, narrow throat, steep running, cross & landing slopes, narrow landing & traversable path |
| 5       | V            | 2        | Yes              | 536 | Winesap & Golden    | 3            | No truncated domes, narrow throat, narrow landing traversable path  |
| 5       | V            | 3        | N/A              |     |                     |              |   |
| 5       | V            | 4        | N/A              |     |                     |              |   |
| 5       | W            | 1        | Yes              | 534 | Rome & Applewood    | 3            | No truncated domes, narrow throat, narrow landing traversable path  |
| 5       | W            | 2        | Yes              | 533 | Rome & Applewood    | 3            | No truncated domes, narrow throat, narrow landing traversable path  |
| 5       | W            | 3        | N/A              |     |                     |              |   |
| 5       | W            | 4        | N/A              |     |                     |              |   |
| 5       | X            | 1        | Yes              | 531 | Rome & Golden       | 3            | No truncated domes, narrow throat, steep running slope, narrow landing & traversable path                   |
| 5       | X            | 2        | Yes              | 532 | Rome & Golden       | 3            | No truncated domes, narrow throat, steep running & cross slope, narrow landing & traversable path           |
| 5       | X            | 3        | N/A              |     |                     |              |   |
| 5       | X            | 4        | N/A              |     |                     |              |   |
| 5       | Y            | 1        | Yes              | 530 | Applewood & Golden  | 3            | No truncated domes, narrow throat, steep running slope, narrow landing & traversable path                   |
| 5       | Y            | 2        | Yes              | 529 | Applewood & Golden  | 3            | No truncated domes, narrow throat, steep running & landing slope, narrow landing & traversable path         |
| 5       | Y            | 3        | N/A              |     |                     |              |   |
| 5       | Y            | 4        | N/A              |     |                     |              |   |

## EXISTING CONDITIONS - PEDESTRIAN RAMPS

| Section | Intersection | Corner # | Pedestrian Ramp? | GPS | Location               | Rating (1-3) | Notes   |
|---------|--------------|----------|------------------|-----|------------------------|--------------|---|
| 5       | Z            | 1        | Yes              | 517 | Applewood & Braeburn   | 3            | No truncated domes, no landing or traversable path, steep cross slope           |
| 5       | Z            | 2        | Yes              | 516 | Applewood & Braeburn   | 2            | No truncated domes, no landing or traversable path                              |
| 5       | Z            | 3        | N/A              |     |                        |              |   |
| 5       | Z            | 4        | N/A              |     |                        |              |   |
| 5       | AA           | 1        | Yes              | 515 | Applewood & Jonathan   | 3            | No truncated domes, no landing or traversable path, steep running & cross slope |
| 5       | AA           | 2        | Yes              | 514 | Applewood & Jonathan   | 3            | No truncated domes, no landing or traversable path, steep running slope         |
| 5       | AA           | 3        | N/A              |     |                        |              |   |
| 5       | AA           | 4        | N/A              |     |                        |              |   |
| 5       | BB           | 1        | Yes              | 513 | Applewood & NW 24th    | 2            | No truncated domes, no landing or traversable path                              |
| 5       | BB           | 2        | Yes              | 512 | Applewood & NW 24th    | 2            | No truncated domes, no landing or traversable path                              |
| 5       | BB           | 3        | Yes              | 511 | Applewood & NW 24th    | 2            | No truncated domes  |
| 5       | BB           | 4        | Yes              | 510 | Applewood & NW 24th    | 2            | No truncated domes  |
| 5       | CC           | 1        | Yes              | 508 | Applewood & NW 23rd    | 2            | No truncated domes  |
| 5       | CC           | 2        | Yes              | 507 | Applewood & NW 23rd    | 2            | No truncated domes  |
| 5       | CC           | 3        | Yes              | 506 | Applewood & NW 23rd    | 2            | No truncated domes  |
| 5       | CC           | 4        | Yes              | 509 | Applewood & NW 23rd    | 2            | No truncated domes  |
| 5       | DD           | 1        | Yes              | 505 | Applewood & NW 22nd    | 2            | No truncated domes  |
| 5       | DD           | 2        | Yes              | 504 | Applewood & NW 22nd    | 2            | No truncated domes  |
| 5       | DD           | 3        | N/A              |     |                        |              |   |
| 5       | DD           | 4        | N/A              |     |                        |              |   |
| 5       | EE           | 1        | Yes              | 503 | Bishop & NW 22nd       | 2            | No truncated domes  |
| 5       | EE           | 2        | Yes              | 502 | Bishop & NW 22nd       | 2            | No truncated domes  |
| 5       | EE           | 3        | N/A              |     |                        |              |   |
| 5       | EE           | 4        | N/A              |     |                        |              |   |
| 5       | FF           | 1        | Yes              | 501 | Bishop & Shamrock      | 1            |   |
| 5       | FF           | 2        | Yes              | 500 |                        | 1            |   |
| 5       | FF           | 3        | N/A              |     |                        |              |   |
| 5       | FF           | 4        | N/A              |     |                        |              |   |
| 5       | GG           | 1        | Yes              | 499 | Alpine & 21st          | 3            | steep cross & landing slopes  |
| 5       | GG           | 2        | Yes              | 498 | Alpine & 21st          | 1            |   |
| 5       | GG           | 3        | N/A              |     |                        |              |   |
| 5       | GG           | 4        | N/A              |     |                        |              |   |
| 5       | HH           | 1        | Yes              | 497 | Spring Creek & NW 21st | 1            |   |
| 5       | HH           | 2        | Yes              | 496 | Spring Creek & NW 21st | 3            | steep cross & landing slopes  |
| 5       | HH           | 3        | N/A              |     |                        |              |   |
| 5       | HH           | 4        | N/A              |     |                        |              |   |
| 5       | II           | 1        | Yes              | 494 | Bishop & NW 21st       | 3            | steep landing & cross slopes, poor drop-off at flow line                        |
| 5       | II           | 2        | Yes              | 495 | Bishop & NW 21st       | 3            | steep landing slope   |
| 5       | II           | 3        | Yes              | 492 | Timber Creek & NW 21st | 3            | steep landing slope   |
| 5       | II           | 4        | Yes              | 493 | Timber Creek & NW 21st | 3            | steep landing & cross slopes  |
| 5       | JJ           | 1        | Yes              | 491 | Copper Creek & NW 21st | 3            | steep landing & cross slopes  |
| 5       | JJ           | 2        | Yes              | 490 | Copper Creek & NW 21st | 3            | steep landing slope   |
| 5       | JJ           | 3        | N/A              |     |                        |              |   |
| 5       | JJ           | 4        | N/A              |     |                        |              |   |
| 5       | KK           | 1        | Yes              | 488 | N Whitley & NW 21st    | 3            | Steep cross slope, narrow landing & traversable path                            |
| 5       | KK           | 2        | Yes              | 577 | N Whitley & NW 21st    | 3            | No truncated domes, steep running slope   |
| 5       | KK           | 3        | Yes              | 576 | N Whitley & NW 21st    | 2            | No truncated domes, narrow landing & traversable path                           |
| 5       | KK           | 4        | Yes              | 489 | N Whitley & NW 21st    | 2            | No truncated domes  |

# **Appendix C:**

## **Existing Conditions – Sidewalks Table**

| EXISTING CONDITIONS - SIDEWALKS |              |                       |                       |      |            |   |
|---------------------------------|--------------|-----------------------|-----------------------|------|------------|---|
| Section                         | Street       | From                  | To                    | Side | Compliant? | Notes   |
| 1                               | S Kansas     | S Whitley             | Fruitland High School | A    | No         | Sidewalk in good condition, but contains non-compliant driveway approaches                    |
|                                 |              |                       |                       | B    | No         | Sidewalk in good condition, but contains non-compliant driveway approaches                    |
| 1                               | S Kansas     | Fruitland High School | SW 4th                | A    | N/A        | No sidewalk   |
|                                 |              |                       |                       | B    | N/A        | No sidewalk   |
| 1                               | Ayers        | S Whitley             | Dead End              | A    | N/A        | No sidewalk   |
|                                 |              |                       |                       | B    | N/A        | No sidewalk   |
| 1                               | SW 8th       | S Whitley             | S Whitley Ct          | A    | Yes        |   |
|                                 |              |                       |                       | B    | N/A        |   |
| 1                               | S Whitley Ct | SW 8th                | Dead End              | A    | No         | Sidewalk in poor condition: Entrance to underground pathway has steep running slope (GPS 192) |
|                                 |              |                       |                       | B    | Yes        |   |
| 1                               | SW 8th       | S Whitley Ct          | 3 Rivers Way          | A    | Yes        |   |
|                                 |              |                       |                       | B    | N/A        |   |
| 1                               | SW 9th       | Vermont               | S Arizona             | A    | N/A        | No sidewalk   |
|                                 |              |                       |                       | B    | N/A        | No sidewalk   |
| 1                               | W 10th       | Vermont               | S Arizona             | A    | N/A        | No sidewalk   |
|                                 |              |                       |                       | B    | N/A        | No sidewalk   |
| 1                               | S Arizona    | SW 9th                | SW 8th                | A    | N/A        | No sidewalk   |
|                                 |              |                       |                       | B    | N/A        | No sidewalk   |
| 1                               | S Washington | SW 9th                | SW 8th                | A    | N/A        | No sidewalk   |
|                                 |              |                       |                       | B    | N/A        | No sidewalk   |
| 1                               | S Washington | SW 8th                | Cornwall              | A    | Yes        |   |
|                                 |              |                       |                       | B    | Yes        |   |
| 1                               | Cornwall     |                       |                       | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | Chadman      | Cornwall Way          | Dead End              | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | S Meadows    | SW 8th                | Partridge             | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | Partridge    | Pintail               | Meadow Lark           | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | Pintail      | partridge             | S Whitetail           | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | S Whitetail  | Pintail               | Meadow Lark           | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | Meadow lark  | partridge             | Redwing               | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | Mallard      | Meadow Lark           | Redwing               | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | Redwing      | Meadow Lark           | Whitetail             | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | Whitetail    | Red Wing              | Mallard               | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | Meadows      | Red Wing              | Wood Duck             | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | Wood Duck    | 3 Rivers Way          | Dead End              | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | N Whitetail  | Wood Duck             | Dead End              | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | 3 Rivers Way | Wood Duck             | SW 8th                | A    | Yes        | rolled curb   |
|                                 |              |                       |                       | B    | Yes        | rolled curb   |
| 1                               | SW 7th       | S Whitley             | S Colorado            | A    | N/A        | No sidewalk   |
|                                 |              |                       |                       | B    | N/A        | No sidewalk   |

| EXISTING CONDITIONS - SIDEWALKS |                |            |                |      |            |  |
|---------------------------------|----------------|------------|----------------|------|------------|--|
| Section                         | Street         | From       | To             | Side | Compliant? | Notes  |
| 2                               | S Pennsylvania | US 95      | SW 4th         | A    | Yes        |  |
|                                 |                |            |                | B    | Yes        |  |
| 2                               | S Pennsylvania | SW4th      | SW 3rd         | A    | N/A        |  |
|                                 |                |            |                | B    | N/A        |  |
| 2                               | SW 7th         | S Iowa     | S Pennsylvania | A    | N/A        |  |
|                                 |                |            |                | B    | N/A        |  |
| 2                               | S Minnesota    | SW 7th     | S 5th          | A    | N/A        |  |
|                                 |                |            |                | B    | N/A        |  |
| 2                               | S Minnesota    | S 5th      | SW 3rd         | A    | No         | Sidewalk in poor condition: discontinuities, not continuous  |
|                                 |                |            |                | B    | No         | Sidewalk in poor condition: discontinuities, not continuous  |
| 2                               | S Iowa         | SW 7th     | SW 4th         | A    | No         | Sidewalk in poor condition: not continuous along street, approaches between 5th & 4th                          |
|                                 |                |            |                | B    | No         | Sidewalk in poor condition: not continuous along street  |
| 2                               | S Iowa         | SW 4th     | SW 3rd         | A    | N/A        |  |
|                                 |                |            |                | B    | N/A        |  |
| 2                               | S Iowa         | SW 3rd     | W 1st          | A    | N/A        |  |
|                                 |                |            |                | B    | N/A        |  |
| 2                               | S Nebraska     | SW 4th     | SW 3rd         | A    | No         | Sidewalk in poor condition: not continuous along street, narrow  |
|                                 |                |            |                | B    | N/A        |  |
| 2                               | S Nebraska     | SW 3rd     | SW 2nd         | A    | No         | Sidewalk in poor condition: not continuous along street, narrow  |
|                                 |                |            |                | B    | No         | Sidewalk in poor condition: not continuous along street  |
| 2                               | S Nebraska     | SW 2nd     | W 1st          | A    | No         | Sidewalk in poor condition: not continuous along street  |
|                                 |                |            |                | B    | N/A        |  |
| 2                               | S Kansas       | SW 4th     | SW 3rd         | A    | N/A        |  |
|                                 |                |            |                | B    | No         | Sidewalk in poor condition: not continuous along street, narrow  |
| 2                               | S Kansas       | SW 3rd     | SW 2nd         | A    | No         | Sidewalk in poor condition: not continuous along street, narrow  |
|                                 |                |            |                | B    | Yes        |  |
| 2                               | S Kansas       | SW 2nd     | W 1st          | A    | N/A        |  |
|                                 |                |            |                | B    | N/A        |  |
| 2                               | S Colorado     | SW 7th     | SW 3rd         | A    | N/A        |  |
|                                 |                |            |                | B    | N/A        |  |
| 2                               | S Utah         | SW 7th     | SW 4th         | A    | N/A        |  |
|                                 |                |            |                | B    | N/A        |  |
| 2                               | SW 4th         | S Whitley  | S Nebraska     | A    | N/A        |  |
|                                 |                |            |                | B    | N/A        |  |
| 2                               | SW 4th         | S Nebraska | S Pennsylvania | A    | No         | Sidewalk in poor condition: Narrow , in poor condition, not continuous   |
|                                 |                |            |                | B    | No         | Sidewalk in poor condition: Narrow , in poor condition, not continuous   |
| 2                               | SW 3rd         | S Whitley  | S Colorado     | A    | No         | Non-compliant driveway approaches  |
|                                 |                |            |                | B    | No         | Sidewalk in poor condition: not continuous   |
| 2                               | SW 3rd         | S Colorado | S Kansas       | A    | Yes        | Northern sidewalk near park in good condition  |
|                                 |                |            |                | B    | No         | Sidewalk in poor condition   |
| 2                               | SW 3rd         | S Kansas   | S Minnesota    | A    | No         | Sidewalk in poor condition   |
|                                 |                |            |                | B    | No         | Sidewalk in poor condition   |
| 2                               | SW 2nd         | S Whitley  | S Kansas       | A    | N/A        |  |
|                                 |                |            |                | B    | No         | Sidewalk in poor condition: not continuous, in good condition near park, but non-compliant driveway approaches |

| EXISTING CONDITIONS - SIDEWALKS |             |                |                |      |            |                                    |
|---------------------------------|-------------|----------------|----------------|------|------------|------------------------------------|
| Section                         | Street      | From           | To             | Side | Compliant? | Notes                              |
| 2                               | SW 2nd      | S Kansas       | S Nebraska     | A    | N/A        |                                    |
|                                 |             |                |                | B    | N/A        |                                    |
| 2                               | SW 2nd      | S Nebraska     | S Iowa         | A    | No         | Sidewalk in poor condition: Narrow |
|                                 |             |                |                | B    | No         |                                    |
| 2                               | W 1st       | S Whitley      | S Pennsylvania | A    | N/A        |                                    |
|                                 |             |                |                | B    | N/A        |                                    |
| 2                               | E 1st       | S Pennsylvania | Paradise Ct    | A    | N/A        |                                    |
|                                 |             |                |                | B    | N/A        |                                    |
| 2                               | Paradise ct | E 1st          | Dead End       | A    | Yes        | No sidewalk at end of cul-de-sac   |
|                                 |             |                |                | B    | Yes        | No sidewalk at end of cul-de-sac   |

|   |                  |                  |                |   |     |  |
|---|------------------|------------------|----------------|---|-----|--|
| 3 | N Pennsylvania   | W 1st            | NW 4th         | A | Yes | Sidewalk not along entire stretch of road, Non-compliant driveway approaches |
|   |                  |                  |                | B | N/A |  |
| 3 | N Pennsylvania   | NW 4th           | NW 8th         | A | No  | Non-compliant driveway approaches  |
|   |                  |                  |                | B | N/A |  |
| 3 | N Pennsylvania   | NW 8th           | NW 9th         | A | N/A |  |
|   |                  |                  |                | B | N/A |  |
| 3 | Syringia Falls   | N Pennsylvania   | Dead End       | A | Yes |  |
|   |                  |                  |                | B | Yes |  |
| 3 | Magnolia         | N Pennsylvania   | Zinnia         | A | Yes |  |
|   |                  |                  |                | B | Yes |  |
| 3 | Syringia Springs | Magnolia         | Dead End       | A | Yes |  |
|   |                  |                  |                | B | Yes |  |
| 3 | Honey Suckle     | Syringia Springs | Dead End       | A | Yes |  |
|   |                  |                  |                | B | Yes |  |
| 3 | Lilac            | Magnolia         | Honey Suckle   | A | Yes |  |
|   |                  |                  |                | B | Yes |  |
| 3 | Orchid           | Syringia Springs | Dead End       | A | Yes |  |
|   |                  |                  |                | B | Yes |  |
| 3 | Zinnia           | Magnolia         | Dead End       | A | Yes |  |
|   |                  |                  |                | B | Yes |  |
| 3 | NE 8th           | N Pennsylvania   | Dead End       | A | N/A |  |
|   |                  |                  |                | B | Yes |  |
| 3 | NW 9th           | N Pennsylvania   | Dead End       | A | No  | Non-compliant driveway approaches  |
|   |                  |                  |                | B | No  | Non-compliant driveway approaches  |
| 3 | W 1st            | N Whitley        | Wild Rose      | A | No  | Non-compliant driveway approaches  |
|   |                  |                  |                | B | N/A |  |
| 3 | Wild Rose        | W 1st            | Dead End       | A | No  | Non-compliant driveway approaches  |
|   |                  |                  |                | B | No  | Non-compliant driveway approaches  |
| 3 | N Utah           | W 1st            | NW 2nd         | A | N/A |  |
|   |                  |                  |                | B | No  | Non-compliant driveway approaches  |
| 3 | N Utah           | NW 2nd           | Dead End       | A | No  | Non-compliant driveway approaches  |
|   |                  |                  |                | B | No  | Non-compliant driveway approaches  |
| 3 | N Park           | NW 2nd           | Dead End       | A | N/A |  |
|   |                  |                  |                | B | N/A |  |
| 3 | NW 2nd           | N Utah           | N Park         | A | Yes |  |
|   |                  |                  |                | B | Yes |  |
| 3 | NW 2nd           | N Park           | N Kansas       | A | N/A |  |
|   |                  |                  |                | B | No  |  |
| 3 | NW 3rd           | N Whitley        | N Kansas       | A | N/A |  |
|   |                  |                  |                | B | N/A |  |
| 3 | NW 3rd           | N Kansas         | Crimson Circle | A | No  | Non-compliant driveway approaches  |
|   |                  |                  |                | A | No  | Non-compliant driveway approaches  |
| 3 | NW 4th           | N Whitley        | Dead End       | A | N/A |  |
|   |                  |                  |                | B | N/A |  |
| 3 | N Utah           | NW 4th           | N Whitley      | A | N/A |  |
|   |                  |                  |                | B | N/A |  |
| 3 | North Dakota     | NW 4th           | NW 2nd         | A | N/A |  |
|   |                  |                  |                | B | N/A |  |
| 3 | N Kansas         | NW 4th           | W 1st          | A | N/A |  |
|   |                  |                  |                | B | N/A |  |
| 3 | Crimson Circle   | NW 4th           | Dead End       | A | No  | Non-compliant driveway approaches  |
|   |                  |                  |                | B | No  | Non-compliant driveway approaches  |
| 3 | E Crimson Circle | Crimson Circle   | Dead End       | A | Yes |  |
|   |                  |                  |                | B | Yes |  |

## EXISTING CONDITIONS - SIDEWALKS

| Section | Street       | From       | To         | Side | Compliant? | Notes                             |
|---------|--------------|------------|------------|------|------------|-----------------------------------|
| 3       | NW 2nd       | N Whitley  | Dead End   | A    | No         | Non-compliant driveway approaches |
|         |              |            |            | B    | No         | Non-compliant driveway approaches |
| 3       | Orchard Park | N Whitley  | Washington | A    | N/A        |                                   |
|         |              |            |            | B    | N/A        |                                   |
| 3       | NW 3rd       | N Whitley  | Washington | A    | N/A        |                                   |
|         |              |            |            | B    | N/A        |                                   |
| 3       | NW 3rd       | Washington | Dead End   | A    | Yes        |                                   |
|         |              |            |            | B    | Yes        |                                   |
| 3       | Bobwhite     |            |            | A    | Yes        |                                   |
|         |              |            |            | B    | Yes        |                                   |
| 3       | Quail Cove   | Bobwhite   | Dead End   | A    | Yes        |                                   |
|         |              |            |            | B    | Yes        |                                   |
| 3       | Washington   | NW 3rd     | NW 2nd     | A    | N/A        |                                   |
|         |              |            |            | B    | Yes        |                                   |
| 3       | NW 7th       | N Whitley  | Dead End   | A    | Yes        |                                   |
|         |              |            |            | B    | Yes        |                                   |

|   |                |                |                |   |     |                                   |
|---|----------------|----------------|----------------|---|-----|-----------------------------------|
| 4 | NW 16th        | N Whitley      | Ash Loop       | A | No  | Non-compliant driveway approaches |
|   |                |                |                | B | No  | Sidewalk not continuous           |
| 4 | NW 16th        | Ash Loop       | N Pennsylvania | A | Yes |                                   |
|   |                |                |                | B | N/A |                                   |
| 4 | NE 16th        | N Pennsylvania | Lucille        | A | N/A |                                   |
|   |                |                |                | B | No  | Non-compliant driveway approaches |
| 4 | NE 16th        | Lucille        | Jessica        | A | N/A |                                   |
|   |                |                |                | B | N/A |                                   |
| 4 | N Pennsylvania | NW 9th         | NE 12th        | A | N/A |                                   |
|   |                |                |                | B | N/A |                                   |
| 4 | N Pennsylvania | NE 12th        | NE 14th        | A | No  | Non-compliant driveway approaches |
|   |                |                |                | B | N/A |                                   |
| 4 | N Pennsylvania | NE 14th        | NE 16th        | A | N/A |                                   |
|   |                |                |                | B | N/A |                                   |
| 4 | N Pennsylvania | NE 16th        | Oak            | A | N/A |                                   |
|   |                |                |                | B | Yes |                                   |
| 4 | NW 11th        | N Whitley      | N Vermont      | A | No  | Non-compliant driveway approaches |
|   |                |                |                | B | N/A |                                   |
| 4 | NW 12th        | N Whitley      | N Vermont      | A | No  | Non-compliant driveway approaches |
|   |                |                |                | B | No  | Non-compliant driveway approaches |
| 4 | N Vermont      | NW 11th        | NW 12th        | A | N/A |                                   |
|   |                |                |                | B | No  | Non-compliant driveway approaches |
| 4 | NW 13th        | N Whitley      | Dead End       | A | No  | Non-compliant driveway approaches |
|   |                |                |                | B | No  | Non-compliant driveway approaches |
| 4 | NE 11th        | N Pennsylvania | Dead End       | A | N/A |                                   |
|   |                |                |                | B | N/A |                                   |
| 4 | NE 12th        | N Pennsylvania | Chaney         | A | No  | Non-compliant driveway approaches |
|   |                |                |                | B | No  | Non-compliant driveway approaches |
| 4 | NE 14th        | N Pennsylvania | Burton         | A | No  | Non-compliant driveway approaches |
|   |                |                |                | B | No  | Non-compliant driveway approaches |
| 4 | NE 14th        | Burton         | Lucille        | A | No  | Non-compliant driveway approaches |
|   |                |                |                | B | N/A |                                   |
| 4 | NE 14th        | Lucille        | Chaney         | A | No  | Non-compliant driveway approaches |
|   |                |                |                | B | N/A |                                   |
| 4 | Chaney         | NE 14th        | NE 12th        | A | No  | Non-compliant driveway approaches |
|   |                |                |                | B | N/A |                                   |
| 4 | NE 13th        | Chaney         | Tara           | A | Yes |                                   |
|   |                |                |                | B | Yes |                                   |
| 4 | NE 13th        | Tara           | Jessica        | A | Yes |                                   |
|   |                |                |                | B | Yes |                                   |
| 4 | Tara           |                |                | A | No  | Non-compliant driveway approaches |
|   |                |                |                | B | No  | Non-compliant driveway approaches |
| 4 | Jessica        |                |                | A | Yes |                                   |
|   |                |                |                | B | Yes |                                   |
| 4 | Lucille        | NE 14th        | NE 16th        | A | No  | sidewalk in poor condition        |
|   |                |                |                | B | No  | sidewalk in poor condition        |
| 4 | 15th Cir       | Lucille        | Dead End       | A | No  | sidewalk in poor condition        |
|   |                |                |                | B | No  | sidewalk in poor condition        |

| EXISTING CONDITIONS - SIDEWALKS |                     |                |           |      |            |  |
|---------------------------------|---------------------|----------------|-----------|------|------------|--|
| Section                         | Street              | From           | To        | Side | Compliant? | Notes  |
| 4                               | Burton              | NE 16th        | NE 14th   | A    | No         | sidewalk in poor condition                       |
|                                 |                     |                |           | B    | No         | sidewalk in poor condition                       |
| 4                               | Ash Loop            | NW 16th        | Dead End  | A    | N/A        |  |
|                                 |                     |                |           | B    | N/A        |  |
| 4                               | Desert Rose Estates | NW 16th        | Dead End  | A    | N/A        |  |
|                                 |                     |                |           | B    | N/A        |  |
| 4                               | Oak                 | N Pennsylvania |           | A    | Yes        |  |
|                                 |                     |                |           | B    | Yes        |  |
| 4                               | Beech               | Oak            | Hickory   | A    | Yes        |  |
|                                 |                     |                |           | B    | Yes        |  |
| 4                               | Hickory             | Beech          | Sycamore  | A    | Yes        |  |
|                                 |                     |                |           | B    | Yes        |  |
| 4                               | Sycamore            | Hickory        | Oak       | A    | Yes        |  |
|                                 |                     |                |           | B    | Yes        |  |
| 4                               | Walnut              | Beech          | Dead End  | A    | Yes        |  |
|                                 |                     |                |           | B    | Yes        |  |
| 5                               | NW 16th             | N Whitley      | Allen     | A    | No         | Non-compliant driveway approaches                |
|                                 |                     |                |           | B    | No         | Non-compliant driveway approaches                |
| 5                               | Allen               | NW 16th        | Washoe    | A    | No         | Non-compliant driveway approaches                |
|                                 |                     |                |           | B    | No         | Non-compliant driveway approaches                |
| 5                               | NW 20th             | Allen          |           | A    | N/A        |  |
|                                 |                     |                |           | B    | N/A        |  |
| 5                               | Heitz               | NW 20th        | Dead End  | A    | No         | sidewalk at end of cul-de-sac, in poor condition |
|                                 |                     |                |           | B    | No         | sidewalk at end of cul-de-sac, in poor condition |
| 5                               | NW 19th             | Allen          |           | A    | N/A        |  |
|                                 |                     |                |           | B    | N/A        |  |
| 5                               | NW 18th Circle      | Allen          | Dead End  | A    | No         | Sidewalk in poor condition                       |
|                                 |                     |                |           | B    | No         | Sidewalk in poor condition                       |
| 5                               | NW 17th             | Allen          |           | A    | N/A        |  |
|                                 |                     |                |           | B    | N/A        |  |
| 5                               | N Arizona           | NW 16th        |           | A    | Yes        |  |
|                                 |                     |                |           | B    | Yes        |  |
| 5                               | Washoe              | Allen          | Poplar    | A    | N/A        |  |
|                                 |                     |                |           | B    | N/A        |  |
| 5                               | Poplar              | Washoe         | Spruce    | A    | Yes        |  |
|                                 |                     |                |           | B    | Yes        |  |
| 5                               | Dogwood             |                |           | A    | Yes        |  |
|                                 |                     |                |           | B    | Yes        |  |
| 5                               | Bayberry            | Poplar         | Dead End  | A    | Yes        |  |
|                                 |                     |                |           | B    | Yes        |  |
| 5                               | Birch               | Spruce         | Dead End  | A    | Yes        |  |
|                                 |                     |                |           | B    | Yes        |  |
| 5                               | Spruce              | Poplar         | Birch     | A    | Yes        |  |
|                                 |                     |                |           | B    | Yes        |  |
| 5                               | Spruce              | Birch          | N Whitley | A    | No         | Non-compliant driveway approaches                |
|                                 |                     |                |           | B    | N/A        |  |
| 5                               | NW 24th             | Washoe         | Applewood | A    | N/A        |  |
|                                 |                     |                |           | B    | Yes        | Good where sidewalk exists                       |
| 5                               | NW 24th             | Applewood      | Golden    | A    | No         | Non-compliant driveway approaches                |
|                                 |                     |                |           | B    | Yes        |  |

| EXISTING CONDITIONS - SIDEWALKS |           |              |              |      |            |                                   |
|---------------------------------|-----------|--------------|--------------|------|------------|-----------------------------------|
| Section                         | Street    | From         | To           | Side | Compliant? | Notes                             |
| 5                               | NW 24th   | Golden       | N Whitley    | A    | No         | Non-compliant driveway approaches |
|                                 |           |              |              | B    | No         | Non-compliant driveway approaches |
| 5                               | NW 21st   | N Whitley    | Copper Creek | A    | Yes        |                                   |
|                                 |           |              |              | B    | No         | Non-compliant driveway approaches |
| 5                               | NW 21st   | Copper Creek | Bishop       | A    | Yes        |                                   |
|                                 |           |              |              | B    | Yes        |                                   |
| 5                               | NW 21st   | Bishop       | Spring Creek | A    | Yes        |                                   |
|                                 |           |              |              | B    | Yes        |                                   |
| 5                               | NW 21st   | Spring Creek | Alpine       | A    | Yes        |                                   |
|                                 |           |              |              | B    | Yes        |                                   |
| 5                               | Bishop    | Dead End     | NW 23rd      | A    | Yes        |                                   |
|                                 |           |              |              | B    | Yes        |                                   |
| 5                               | NW 22nd   | Bishop       | Dead End     | A    | Yes        |                                   |
|                                 |           |              |              | B    | Yes        |                                   |
| 5                               | NW 23rd   | Bishop       | Applewood    | A    | Yes        |                                   |
|                                 |           |              |              | B    | Yes        |                                   |
| 5                               | Applewood | NW 22nd      | NW 24th      | A    | Yes        |                                   |
|                                 |           |              |              | B    | Yes        |                                   |
| 5                               | Applewood | NW 24th      | Winesap      | A    | No         | Non-compliant driveway approaches |
|                                 |           |              |              | B    | No         | Non-compliant driveway approaches |
| 5                               | Jonathan  | Applewood    | Golden       | A    | No         | Non-compliant driveway approaches |
|                                 |           |              |              | B    | No         | Non-compliant driveway approaches |
| 5                               | Braeburn  | Applewood    | Golden       | A    | No         | Non-compliant driveway approaches |
|                                 |           |              |              | B    | No         | Non-compliant driveway approaches |
| 5                               | Golden    | Applewood    | NW 24th      | A    | No         | Non-compliant driveway approaches |
|                                 |           |              |              | B    | No         | Non-compliant driveway approaches |
| 5                               | Rome      | Applewood    | Golden       | A    | No         | Non-compliant driveway approaches |
|                                 |           |              |              | B    | No         | Non-compliant driveway approaches |
| 5                               | Winesap   | Rome         | Golden       | A    | No         | Non-compliant driveway approaches |
|                                 |           |              |              | B    | No         | Non-compliant driveway approaches |
| 5                               | Alden Rd  |              |              | A    | N/A        |                                   |
|                                 |           |              |              | B    | N/A        |                                   |

# **Appendix D:**

**Section Maps (1-5)**

**Non-Compliant Driveway Approaches**



Section 1 Map: Non-compliant Driveway Approaches

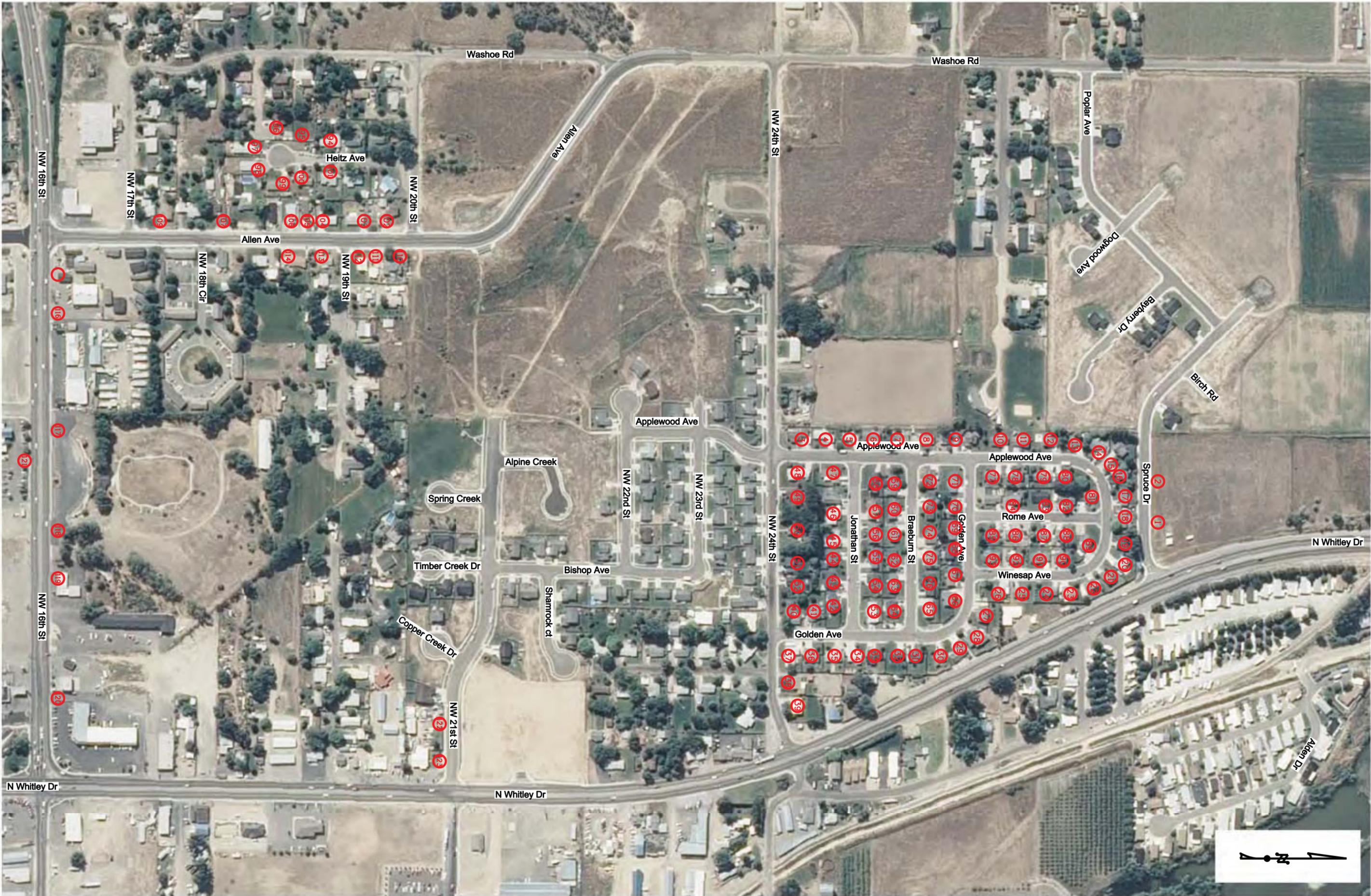


Section 2 Map: Non-compliant Driveway Approaches



Section 3 Map: Non-compliant Driveway Approaches





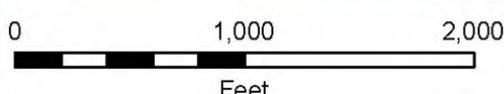
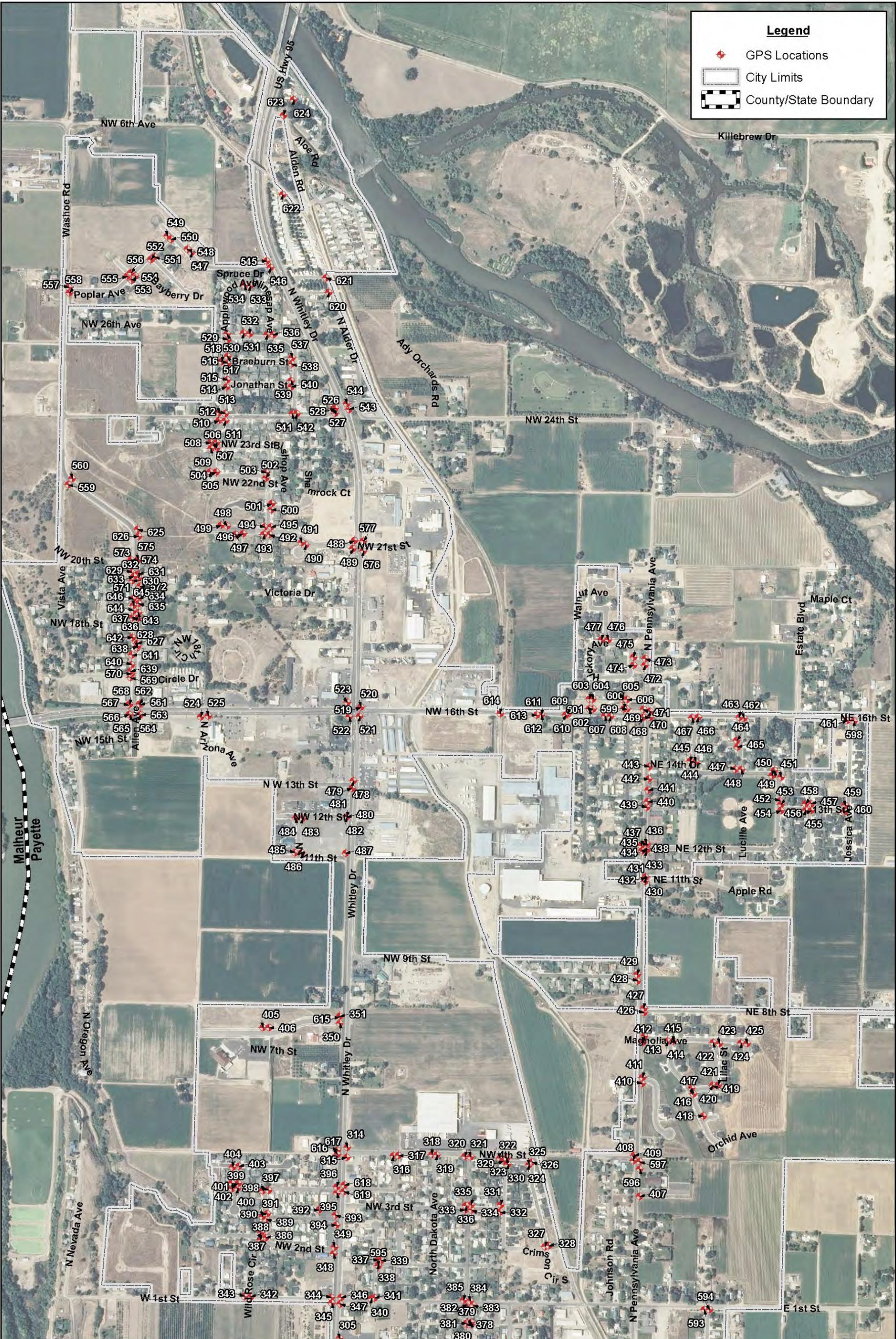
Section 5 Map: Non-compliant Driveway Approaches

# **Appendix E:**

**GPS Data Maps (1-2)**

**Legend**

- GPS Locations
- City Limits
- County/State Boundary

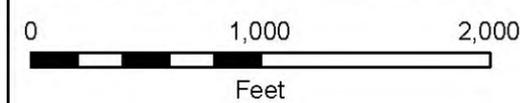
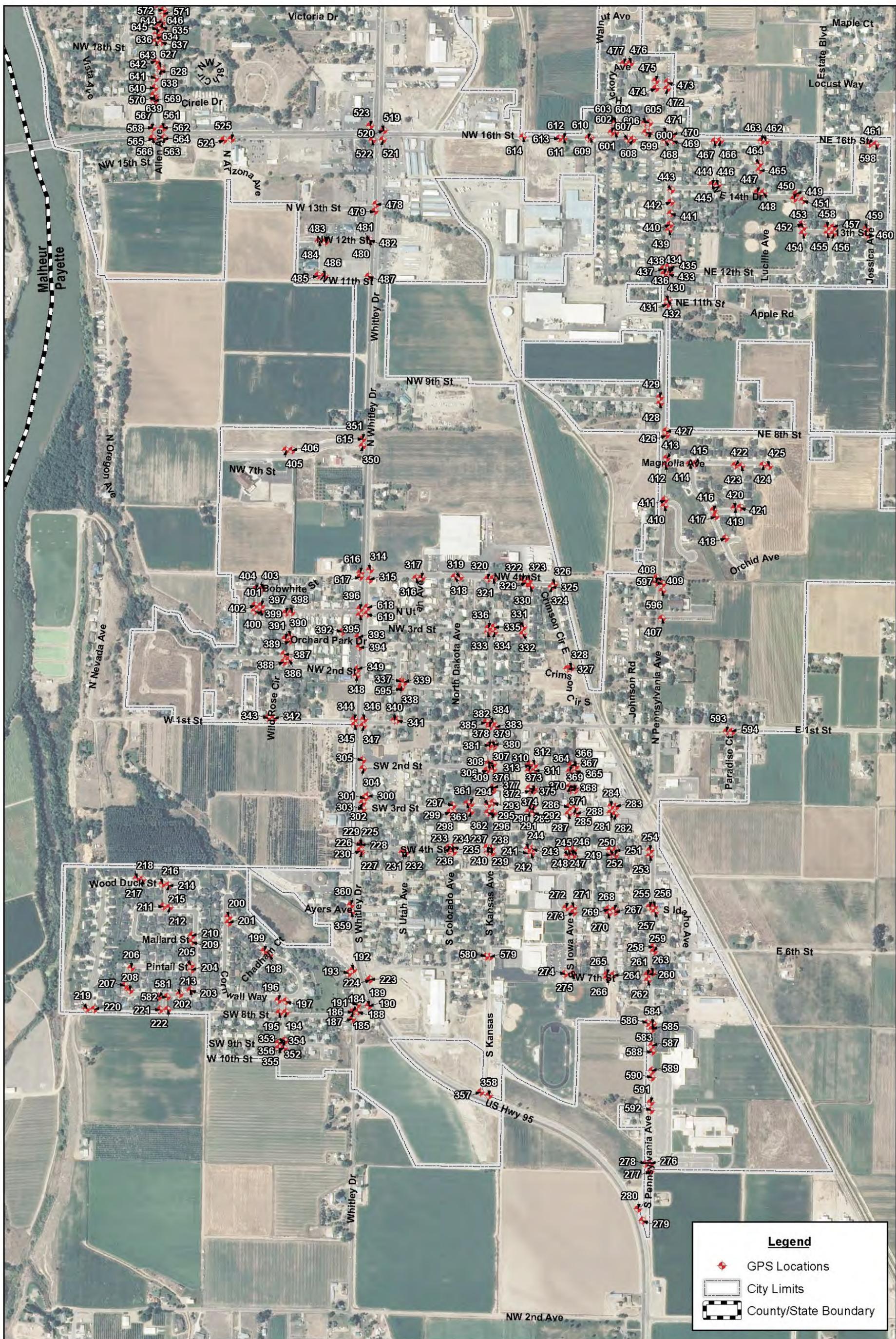


**GPS Data Map 1**  
Fruitland ADA, Fruitland ID



Image Source: 2009 NAIP, 1 meter resolution, USDA  
Other Data Sources: US Census Bureau

Map Date: 07/28/10  
O:\Piepmeyer\Fruitland\GPSData.mxd



**GPS Data Map 2**  
Fruitland ADA, Fruitland ID



Image Source: 2009 NAIP, 1 meter resolution, USDA  
Other Data Sources: US Census Bureau

Map Date: 07/28/10  
O:\Piepmeyer\Fruitland\GPSData.mxd

# **Appendix F:**

## **Cost Estimates**

Fruitland ADA Inventory  
 ADA Improvements - Cost Estimate  
 Date: July 2010

**Pedestrian Ramps**

|                      |       |
|----------------------|-------|
| Area Ramp (SF) =     | 70.00 |
| Area Sidewalk (SF) = | 96.76 |

| Item                                    | Unit | Qty  | Unit Cost | Total Cost    | Notes   |
|---|------|------|-----------|---------------|---|
| Excavation - Ramp                       | CY   | 1.51 | \$ 6.00   | \$ 9.07       | Assume excavation 7" deep                         |
| Excavation - Sidewalk                   | CY   | 1.49 | \$ 6.00   | \$ 8.96       | Assume excavation 5" deep                         |
| 3/4" Aggregate Base - Ramp              | CY   | 0.86 | \$ 10.00  | \$ 8.64       | 4" thick  |
| 3/4" Aggregate Base - Sidewalk          | CY   | 0.60 | \$ 10.00  | \$ 5.97       | 2" thick  |
| Concrete for Pedestrian Ramp            | CY   | 1.08 | \$ 200.00 | \$ 216.05     | 5" thick  |
| 4" Thick Concrete Sidewalk              | SY   | 1.19 | \$ 27.00  | \$ 32.25      | 4" thick  |
| Construction of Pedestrian Ramp (Labor) | EA   | 1    | \$ 600.00 | \$ 600.00     |   |
| 6" Vertical Curb/Gutter (Type A-2)      | FT   | 22   | \$ 16.00  | \$ 352.00     | End to end with 3-4' extra to match at joint line |
| Repair of Private Property/Landscaping  | EA   | 1    | \$ 80.00  | \$ 80.00      | Includes sprinkler, landscaping, etc.             |
| Truncated Domes                         | EA   | 1    | \$ 150.00 | \$ 150.00     |   |
|   |      |      |           | <b>Total:</b> | <b>\$ 1,462.95</b>                                |

**16-FT Driveway Approach**

|             |        |
|-------------|--------|
| Area (SF) = | 298.76 |
|-------------|--------|

| Item                                      | Unit | Qty  | Unit Cost | Total Cost                            | Notes   |
|---|------|------|-----------|---------------------------------------|---|
| Excavation                                | CY   | 6.45 | \$ 6.00   | \$ 38.73                              | Assume excavation 7" deep                         |
| 3/4" Aggregate Base                       | CY   | 3.69 | \$ 10.00  | \$ 36.88                              | 4" thick  |
| Concrete for Driveway Approach            | CY   | 5.53 | \$ 200.00 | \$ 1,106.52                           | 6" thick  |
| Construction of Driveway Approach (Labor) | EA   | 1    | \$ 700.00 | \$ 700.00                             |   |
| 6" Vertical Curb/Gutter (Type A-2)        | FT   | 34   | \$ 16.00  | \$ 544.00                             | End to end with 3-4' extra to match at joint line |
| Repair of Private Property/Landscaping    | EA   | 1    | \$ 200.00 | \$ 200.00                             | Includes sprinkler, landscaping, etc.             |
|   |      |      |           | <b>Total:</b>                         | <b>\$ 2,626.13</b>                                |
|   |      |      |           | <b>Cost per Foot of Throat Width:</b> | <b>\$ 164.13</b>                                  |

**20-FT Driveway Approach**

|             |        |
|-------------|--------|
| Area (SF) = | 342.76 |
|-------------|--------|

| Item                                      | Unit | Qty  | Unit Cost | Total Cost                            | Notes   |
|---|------|------|-----------|---------------------------------------|---|
| Excavation                                | CY   | 7.41 | \$ 6.00   | \$ 44.43                              | Assume excavation 7" deep                         |
| 3/4" Aggregate Base                       | CY   | 4.23 | \$ 10.00  | \$ 42.32                              | 4" thick  |
| Concrete for Driveway Approach            | CY   | 6.35 | \$ 200.00 | \$ 1,269.48                           | 6" thick  |
| Construction of Driveway Approach (Labor) | EA   | 1    | \$ 800.00 | \$ 800.00                             |   |
| 6" Vertical Curb/Gutter (Type A-2)        | FT   | 38   | \$ 16.00  | \$ 608.00                             | End to end with 3-4' extra to match at joint line |
| Repair of Private Property/Landscaping    | EA   | 1    | \$ 250.00 | \$ 250.00                             | Includes sprinkler, landscaping, etc.             |
|   |      |      |           | <b>Total:</b>                         | <b>\$ 3,014.23</b>                                |
|   |      |      |           | <b>Cost per Foot of Throat Width:</b> | <b>\$ 150.71</b>                                  |

**Sidewalks - 5'x5' slab**

|             |    |
|-------------|----|
| Area (SF) = | 25 |
|-------------|----|

| Item                                   | Unit | Qty  | Unit Cost | Total Cost    | Notes                                 |
|--|------|------|-----------|---------------|---------------------------------------|
| Excavation                             | CY   | 0.31 | \$ 6.00   | \$ 1.85       | Assume excavation 4" deep             |
| 3/4" Aggregate Base                    | CY   | 0.15 | \$ 10.00  | \$ 1.54       | 2" thick                              |
| 4" Thick Concrete Sidewalk             | SY   | 2.78 | \$ 27.00  | \$ 75.00      | 4" thick                              |
| Repair of Private Property/Landscaping | EA   | 1    | \$ 20.00  | \$ 20.00      | Includes sprinkler, landscaping, etc. |
|  |      |      |           |               |                                       |
|  |      |      |           |               |                                       |
|  |      |      |           | <b>Total:</b> | <b>\$ 98.40</b>                       |